

Corpus Christi Regional Transportation Authority

IFB No. 2017-FC-04

ADA Bus Stop Improvements Phase VI

ZONE 2

CONSTRUCTION DRAWINGS

EXHIBIT B-2

Prepared By MGM

CALL BEFORE YOU DIG !

PARTICIPANTS REQUEST
48 HOURS NOTICE BEFORE YOU DIG,
DRILL, OR BLAST - STOP AND

DIAL 811



THE LONE STAR
NOTIFICATION COMPANY
AT 1-800-669-8344



CORPUS CHRISTI REGIONAL
TRANSPORTATION AUTHORITY

ADA BUS STOP IMPROVEMENTS ZONE 2 OCTOBER, 2015

PROJECT NO. 2015-S-17

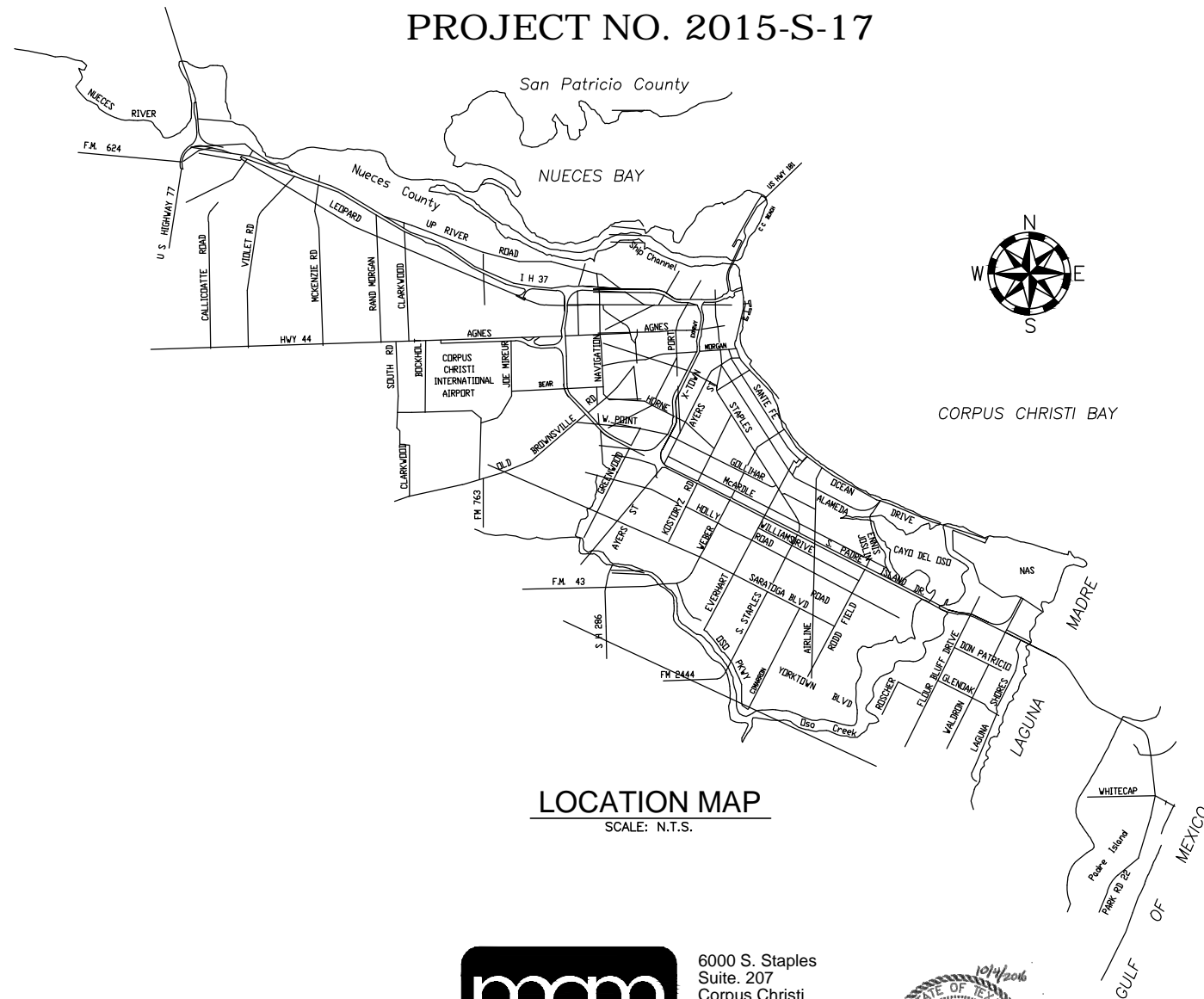
LEGEND

	PROPOSED CONCRETE SHELTER PAD
	PROPOSED PAVEMENT REPAIR
	PROPOSED CONCRETE BUS PAD
	PROPOSED LIMITS OF DEMOLITION
	PROPOSED RAMP
	PROPOSED DETECTABLE WARNING
	REMOVE & REPLACE CURB AND GUTTER
	PROPOSED SIDEWALK
	PROPOSED SOD TO REPLACE DEMO AREA

FL=XX.XX	PROPOSED FLOWLINE ELEVATION
TP=XX.XX	PROPOSED TOP OF PAVEMENT
TC=XX.XX	PROPOSED TOP OF CONCRETE
BC=XX.XX	PROPOSED BACK OF CURB
TS=XX.XX	PROPOSED TOP OF SIDEWALK
FG=XX.XX	PROPOSED FINISH GRADE

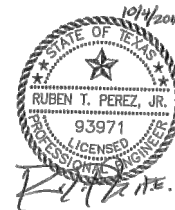
	RIGHT OF WAY
	BENCH MARK LOCATION
	EXISTING BUS SIGN
	EXISTING FIRE HYDRANT
	EXISTING TOPO ELEVATION
	EXISTING GRATE INLET
	EXISTING SANITARY SEWER MANHOLE
	EXISTING STORM SEWER MANHOLE
	EXISTING POWER POLE
	EXISTING SANITARY SEWER LINE
	EXISTING STORM SEWER LINE
	EXISTING WATER LINE
	EXISTING GAS LINE
	EXISTING OVERHEAD LINE
	EXISTING UNDERGROUND ELECTRIC
	EXISTING FENCE LINE

FINAL PLANS



6000 S. Staples
Suite. 207
Corpus Christi
Tx. 78413
P: 361-814-3070
Fax: 888-653-5510

Martinez, Guy & Maybik, Inc.
Civil / Structural Engineering & Surveying Services
TX. Bd. P.E. Firm Reg. No. F-1415



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1. COORDINATION

- A. AGENCIES, DEPARTMENTS, AND FIRMS WHO MAY NEED TO BE CONTACTED THROUGHOUT THE DURATION OF THIS PROJECT
- | | |
|--|--------------|
| REGIONAL TRANSPORTATION AUTHORITY | 361-289-2712 |
| CITY OF CORPUS CHRISTI | 361-826-3500 |
| CITY OF CORPUS CHRISTI TRAFFIC ENGINEERING | 361-826-3500 |
| CITY OF CORPUS CHRISTI MUNICIPAL INFORMATION SYSTEMS | 361-826-3766 |
| TEXAS DEPARTMENT OF TRANSPORTATION | 361-808-2384 |
| MARTINEZ, GUY & MAYBIK, INC. | 361-814-3070 |
| TONY SALINAS (TRAFFIC SIGNAL SUPERINTENDENT) | 361-826-1610 |
| DAVID TREVINO (MIS) | 361-826-3751 |
| DIG-TESS/TEXAS 811 | 800-344-8377 |
| SOUTHWESTERN BELL LOCATE | 800-828-5127 |
| LONE STAR NOTIFICATION | 800-669-8344 |
| TEXAS ONE CALL | 800-545-6005 |
- B. LOCATION AND ADJUSTMENT OF CONFLICTING UTILITIES SHALL BE COORDINATED WITH LOCAL UTILITY AFFECTED. PLANS SHOW INFORMATION OBTAINED FROM SURFACE SURVEY WHICH IS INTENDED AS AN AID FOR THE CONTRACTOR IN DETERMINING APPROXIMATE LOCATION OF CERTAIN LINES. UNDERGROUND UTILITIES DO NOT APPEAR ON THE PLANS BUT MAY EXIST IN THE AREAS OF PROPOSED IMPROVEMENTS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO LOCATE AND PROTECT ALL WORK WITH AGENCIES IN MAKING ALL ADJUSTMENTS REQUIRED BY THE PROJECT. ADJUSTMENTS SHALL BE PERFORMED BY CONTRACTOR OR AFFECTED UTILITY COMPANY WITH NO SEPARATE PAYMENT FOR THIS WORK.
- C. THE CONTRACTOR SHALL NOTIFY THE ENGINEER AND UTILITY COMPANY IF THERE ARE ANY CONFLICTS WITH ANY UTILITIES.
- D. CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL APPLICABLE CONSTRUCTION PERMITS AND FOR PAYING ANY ASSOCIATED FEES.
- E. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO NOTIFY ALL APPROPRIATE UTILITY COMPANIES, CITY TRAFFIC ENGINEERING, & CITY MIS DEPARTMENT 48 HOURS PRIOR TO ACTUAL CONSTRUCTION FOR THE EXACT LOCATION OF EXISTING UTILITIES. THE CONTRACTOR SHALL NOTIFY PROPERTY OWNERS OF PROPOSED CONSTRUCTION IN FRONT OF THE RESPECTIVE PROPERTIES AT LEAST 48 HOURS PRIOR TO START OF CONSTRUCTION. THIS WORK WILL NOT BE PAID FOR DIRECTLY BUT SHALL BE CONSIDERED SUBSIDIARY TO THE PROJECT.
- F. OSHA REGULATIONS PROHIBIT OPERATIONS THAT WILL BRING PERSONS OR EQUIPMENT WITHIN 10 FEET OF AN ENERGIZED ELECTRICAL LINE. WHERE WORKMEN AND/OR EQUIPMENT HAVE TO WORK CLOSE TO AN ENERGIZED ELECTRICAL LINE, THE CONTRACTOR SHALL NOTIFY THE ENGINEER AND THE ENGINEER WILL COORDINATE WITH THE ELECTRICAL POWER COMPANY TO MAKE WHATEVER ADJUSTMENTS ARE NECESSARY TO ENSURE THE SAFETY OF WORKMEN WORKING NEAR THE ENERGIZED LINE. ERECTION AND/OR REMOVAL OF POLES LOCATED NEAR ANY OVERHEAD ELECTRICAL LINES SHALL BE ACCOMPLISHED USING ESTABLISHED INDUSTRY AND UTILITY SAFETY PRACTICES. THE CONTRACTOR SHALL CONSULT WITH THE APPROPRIATE UTILITY COMPANY PRIOR TO BEGINNING SUCH WORK.
2. TRAFFIC CONTROL
- A. THE CONTRACTOR IS RESPONSIBLE FOR DEVELOPING A TRAFFIC CONTROL PLAN TO BE APPROVED BY THE CITY OR STATE PRIOR TO COMMENCING ANY CONSTRUCTION ACTIVITIES. ALL PERMITS AND FEES ASSOCIATED WITH TRAFFIC CONTROL PLAN MUST BE ACQUIRED OR PAID PRIOR TO COMMENCING WORK. ALL FEES ARE SUBSIDIARY TO THE COST OF CONSTRUCTION. CONTRACTOR SHALL PROVIDE 48 HOUR ADVANCE NOTICE, PRIOR TO COMMENCING ANY LANE CLOSURES, TO THE CITY OR STATE TO ALLOW FOR PUBLIC NOTIFICATION OF ANTICIPATED CLOSURES. PAYMENT FOR TRAFFIC CONTROL FOR ANY AWARDED ADD/ALTS SHALL BE INCLUDED IN THE LUMP SUM PAYMENT FOR TRAFFIC CONTROL IN THE BASE BID. NO ADDITIONAL PAYMENTS WILL BE MADE FOR ADD/ALT TRAFFIC CONTROL.
- B. VEHICULAR TRAFFIC TO ADJACENT PROPERTY, HIGHWAYS, PUBLIC ROADS AND STREET CROSSING MUST BE ACCOMMODATED AT ALL SITES DURING CONSTRUCTION. THE CONTRACTORS PLAN FOR ACCOMMODATING TRAFFIC MUST BE SUBMITTED TO AND APPROVED BY THE ENGINEER PRIOR TO DISTURBING OR DEMOLISHING ANY ROADWAY OR PEDESTRIAN SURFACES WITHIN THE LIMITS OF CONSTRUCTION.
- C. ALL WORK IS TO BE COMPLETED BY THE CONTRACTOR DURING DAYLIGHT HOURS. THE CONTRACTOR IS TO PLACE BARRICADES AND BARRELS ADJACENT TO THE WORK SITE. AT THE END OF THE DAY THE CONTRACTOR SHALL OPEN TRAFFIC IF BOTH DIRECTIONS AT ALL INTERSECTIONS. DURING CONSTRUCTION, TRAFFIC CONTROL PLAN TCP (2-4a) OR TCP (1-2b) FOR DAYTIME OPERATION SHALL BE USED. THE ROADWAY SHALL BE REOPENED TO TRAFFIC AT THE END OF EACH WORKDAY.
- D. BARRICADE AND TRAFFIC CONTROL SHALL COMPLY WITH THE CITY OF CORPUS CHRISTI TRAFFIC ENGINEERING DIVISION "UNIFORM BARRICADING STANDARDS AND PRACTICES", TEXAS AND TEXAS DEPARTMENT OF TRANSPORTATION. BARRICADED WORK AREAS SHALL BE LIMITED TO ONE SIDE OF THE BLOCK, EXTENDING FROM CORNER TO CORNER IN ONE DIRECTION, UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER. THE CONTRACTOR MAY BE REQUIRED TO FURNISH ADDITIONAL BARRICADES AND SIGN TO MAINTAIN TRAFFIC AND MOTORIST SAFETY. ANY SUCH ADDITIONAL SIGNS AND BARRICADES SHALL BE CONSIDERED SUBSIDIARY TO THE PROJECT. CONTRACTOR SHALL NOT DETOUR TRAFFIC INTO OPPOSING TRAVEL LANES. CONTRACTOR SHALL PROVIDE PEDESTRIAN TRAFFIC CONTROL FOR CLOSED SIDEWALKS.
- E. THE CONTRACTOR SHALL SUPPLEMENT THE FLAGGER'S LOCATION SHOWN ON THE TRAFFIC CONTROL PLAN SHEETS WITH A TRAILER MOUNTED FLASHING ARROW PANEL.
- F. TRAFFIC CONTROL FOR LANE CLOSURES SHALL BE IN ACCORDANCE WITH THE APPROPRIATE TRAFFIC CONTROL PLAN STANDARD SHEETS.
- G. ANY ADJUSTMENTS REQUIRED TO THE PEDESTRIAN CROSSWALK CONTROL BUTTONS FOR ADA COMPLIANCE WILL BE CONSIDERED SUBSIDIARY TO WORK.

3. DEMOLITION

- A. ALL EXISTING ITEMS IDENTIFIED ON THE PLAN FOR DEMOLITION SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND BE REMOVED AND DISPOSED OF BY THE CONTRACTOR. CONTRACTOR SHALL NOT EXCEED THE LIMITS OF DEMOLITION INDICATED ON THE PLANS.
- B. THE QUANTITIES INDICATED ON THE PLANS WILL BE UTILIZED FOR PAYMENT OF DEMOLITION.
- C. ALL CONCRETE AND ASPHALT SHALL BE SAW-CUT TO FULL DEPTH.
- D. PAVEMENT REPAIR SHALL BE A MAXIMUM OF 2' WIDE (UNLESS OTHERWISE NOTED ON THE PLANS) THE PAVEMENT SHALL BE SAW CUT. THE CONTRACTOR IS RESPONSIBLE FOR PROTECTING THE EDGE OF THE SAW CUT PAVEMENT DURING THE CONSTRUCTION PROCESS. IF THE EDGE IS DAMAGED BY CONSTRUCTION ACTIVITY OR TRAFFIC, THEN THE CONTRACTOR IS RESPONSIBLE FOR CUTTING A NEW CLEAN EDGE. EXTRA PAVEMENT REPAIR RESULTING FROM A DAMAGED EDGE WILL NOT BE PAID FOR.
- E. EXISTING UTILITIES SHOWN ON THE PLANS ARE FOR REFERENCE ONLY AND DO NOT NECESSARILY REPRESENT THE EXACT LOCATION OF SUCH FACILITIES. NOR IS IT IMPLIED THAT ALL EXISTING UTILITIES ARE SHOWN ON THE PLANS. MARTINEZ, GUY & MAYBIK, INC. ASSUMES NO RESPONSIBILITY FOR THE EXISTENCE OR LOCATION OF ANY SUBSURFACE UTILITIES OR STRUCTURES. THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING THE APPROPRIATE UTILITY OWNERS AND LOCATING ALL EXISTING UNDERGROUND AND OVERHEAD UTILITIES PRIOR TO COMMENCING WITH ANY CONSTRUCTION OPERATIONS.
- F. CONTRACTOR SHALL NOT EXCEED THE LIMITS OF DEMOLITION INDICATED ON THE PLANS. EXCESS DEMOLITION WHICH IS DONE BY THE CONTRACTOR WITHOUT THE ENGINEER'S APPROVAL WILL NOT BE PAID FOR. EXTRA CONSTRUCTION RESULTING FROM EXCESS DEMOLITION WILL NOT BE PAID FOR. THE QUANTITIES INDICATED ON THE PLANS WILL BE UTILIZED FOR PAYMENT OF DEMOLITION.
- G. THE CONTRACTOR WILL BE ALLOWED 14 CALENDAR DAYS TO COMPLETE CONSTRUCTION OF DEMOLISHED AREA. IF CONTRACTOR EXCEEDS 14 CALENDAR DAYS AT ANY SITE, NO NEW DEMOLITION WILL BE ALLOWED. ONCE CONTRACTOR HAS COMPLETED PREVIOUSLY DEMOLISHED AREAS, HE MAY RESUME NEW DEMOLITION.

4. CONSTRUCTION

- A. THE CONTRACTOR SHALL VISIT THE PROJECT SITES TO EXAMINE LOCAL CONDITIONS AND PERFORM ACTIONS NECESSARY TO ASSURE THAT THEY UNDERSTAND THE PROJECT THOROUGHLY AND ARE FULLY AWARE OF ALL CONDITIONS AND CONSTRAINTS WHICH MAY BE ENCOUNTERED DURING THE COURSE OF CONSTRUCTION. ALL RIGHT-OF-WAY LINES SHOWN IN THE PLANS ARE APPROXIMATE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE ACTUAL LOCATION OF RIGHT-OF-WAY LINES WITH A REGISTERED PROFESSIONAL LAND SURVEYOR AND ALL WORK SHALL BE PERFORMED WITHIN THE CITY'S RIGHT-OF-WAY. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE PROJECT MANAGER IF PROPOSED IMPROVEMENTS ARE DETERMINED TO BE OUTSIDE OF THE RIGHT-OF-WAY.
- B. THE CONTRACTOR IS RESPONSIBLE FOR STAKING OUT AND CONSTRUCTING THE WORK IN ACCORDANCE WITH REQUIREMENTS OF THE PLANS AND SPECIFICATIONS. CONTRACTOR MAY NOT MAKE ADJUSTMENTS WITHOUT PRIOR APPROVAL OF THE ENGINEER.
- C. CONSTRUCTION OF NEW CURB AND GUTTER SHALL MATCH EXISTING ELEVATIONS AT EACH END OF CURB AND GUTTER, AND SHALL BE SLOPED UNIFORMLY TO PREVENT PONDING. CONSTRUCTION OF NEW CONCRETE SHELTER PAD SHALL MATCH EXISTING/PROPOSED TOP OF STREET CURB AND GUTTER ELEVATION AND SHALL BE SLOPED 2% MAXIMUM/1% MINIMUM TOWARDS CURB AND GUTTER, UNLESS OTHERWISE NOTED. CONSTRUCTION OF NEW CONCRETE SIDEWALK SHALL MATCH EXISTING/PROPOSED TOP OF STREET CURB AND GUTTER ELEVATION WHEN ABUTTING CURB AND GUTTER. CONSTRUCTION OF NEW CONCRETE SIDEWALK SHALL NOT EXCEED 2% CROSS SLOPE, AND 5% RUNNING SLOPE. PURPOSE OF NEW CONCRETE TRANSITIONS IS TO CONNECT PROPOSED ACCESSIBLE ROUTE IMPROVEMENT ELEVATIONS TO THE EXISTING WALKING SURFACES ELEVATIONS. CONCRETE TRANSITIONS SHALL BE CONSIDERED PROJECT IMPROVEMENTS AND SHALL NOT BE CONSIDERED A COMPONENT OF THE ACCESSIBLE ROUTE.
- D. THE CONTRACTOR IS RESPONSIBLE TO MAINTAIN AND NOT BLOCK OR IMPEDE DRAINAGE AND MAINTAIN POSITIVE DRAINAGE FLOW TO PREVENT PONDING WITHIN PROJECT LIMITS. LEVEL LANDINGS SHALL BE SLOPED 2% MAX/1% MIN TOWARDS STREET CURB AND GUTTER. NATURAL GROUND ADJACENT TO PROJECT IMPROVEMENTS SHALL BE GRADED TO DRAIN INTO STREET CURB AND GUTTER.
- E. TOP OF RAMP CURBS (EDGE PROTECTION) ARE INTENDED TO RETAIN NATURAL GROUND AND SHALL MATCH EXIST NATURAL GROUND ELEVATION.
- F. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTMENT OF UTILITY BOXES, MANHOLES ETC., TO MATCH PROPOSED GRADES, AND ADJUSTMENT OF STANDARD SIGNAGE OBSTRUCTING ACCESSIBLE ROUTES. ADJUSTMENT OF UTILITIES, IF NEEDED, WILL BE THE CONTRACTORS RESPONSIBILITY, WITH COORDINATION PROVIDED BY THE ENGINEER AND CONTRACTOR. IF ADJUSTMENTS ARE REQUIRED, CONTRACTOR SHALL NOTIFY ENGINEER.
- G. THE CONTRACTOR SHALL PLACE CLASS "A" (6" MIN. WIDTH) CONCRETE COLLARS AROUND ADJUSTED VALVES AND MANHOLES (12" MIN. WIDTH) AND A MINIMUM 12" DEPTH, ALL AS DIRECTED BY THE ENGINEER.
- H. EXCESS EXCAVATION AND DEMOLISHED MATERIALS WILL BECOME THE PROPERTY OF THE CONTRACTOR AND IS TO BE DISPOSED OF PROPERLY. ANY FILL MATERIAL REQUIRED FOR SUCCESSFUL COMPLETION OF THE PROJECT WILL BE SIMILAR TO THE NATIVE SOILS IN THE AREA IN CLASSIFICATION, GRADATION AND COMPACTION. EXCAVATION AND FILL REQUIRED TO COMPLY WITH SLOPE REQUIREMENTS IS CONSIDERED SUBSIDIARY TO THE PROJECT. CONTRACTOR SHALL REPLACE DISTURBED SOD WITH SAME SPECIES. SITE SHALL BE RESTORED TO ITS ORIGINAL CONDITION OR BETTER.
- I. CLEANING OF ASPHALT EQUIPMENT WILL NOT BE ALLOWED AT THE PROJECT SITE OR IN PUBLIC RIGHT OF WAY. ANY PETROLEUM PRODUCTS SPILLED SHALL BE CLEANED UP AND DISPOSED OF PROPERLY. NO CONSTRUCTION WASTE MATERIALS WILL BE ALLOWED TO BE BURIED ON THE PROPERTY.

- J. IF ANY HAZARDOUS MATERIALS AND/OR CONTAMINATED SOILS ARE DISCOVERED DURING CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY FOR ASSISTANCE IN IDENTIFYING AND TESTING OF MATERIALS AND SOILS.
- K. FIELD CONDITIONS SOMETIMES DICTATE THAT THE LAYOUT BE ADJUSTED. CONTRACTOR MAY NOT MAKE ADJUSTMENTS WITHOUT PRIOR APPROVAL OF THE ENGINEER.
- L. 6" CURB NOT INCLUDED IN RAMP SECTION SHALL BE QUANTIFIED AS 6" HEADER CURB SEPARATELY. 4" HEADER CURB REQUIRED FOR RAMPS/LANDINGS SHALL BE CONSIDERED SUBSIDIARY TO THE RAMP/LANDING PAY ITEMS.
- M. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO PROTECT ALL EXISTING UTILITIES, PIPES, UNDERGROUND STRUCTURES, TRAFFIC SIGNAL BOXES, ELECTRICAL CONDUIT AND CABLES, BUILDINGS, DRIVEWAYS, FENCES AND ALL OTHER PROPERTIES. ALL DAMAGED PROPERTY SHALL BE RESTORED BY THE CONTRACTOR TO ITS ORIGINAL CONDITION ACCORDING TO CITY OF CORPUS CHRISTI STANDARDS, OR BETTER AT NO SEPARATE PAY.
- O. THE CONTRACTOR SHALL HAVE IN PLACE THE NECESSARY STORM WATER SEDIMENT TRAPS AT CURB INLETS AND OPEN DITCH LINES AREAS AS SHOWN IN THE DETAILS WHILE PERFORMING WORK AND UNTIL WORK IS COMPLETE IN THAT AREA AND VEGETATION IS ESTABLISHED. THE CONTRACTOR SHALL UTILIZE BEST MANAGEMENT PRACTICES FOR STORM RUNOFF COMPLYING WITH MS4 PERMIT & TCEQ. TOTAL DISTURBED ACREAGE IS UNDER 1 ACRE.
- P. THE CURB PORTION OF THE STREET CURB & GUTTER SHALL BE PAINTED YELLOW PER SPECIFICATION 025813. THE PAINTING LIMITS SHALL EXTEND 20' BEFORE THE PAD, THRU THE LENGTH OF THE PAD (30' TYPICAL), AND 10' AFTER THE PAD. LENGTH OF PAINTED CURB MAY BE CHANGED IN THE FIELD BY THE ENGINEER DUE TO PHYSICAL CONSTRAINTS. PAYMENT FOR PAINTING SHALL BE PER ACTUAL LENGTH MEASURED IN PLACE.
- Q. ADJUSTMENTS OF VALVES AND/OR METERS WILL BE IDENTIFIED IN THE PLANS, AND SHALL BE CONSIDERED SUBSIDIARY TO THE VARIOUS BID ITEMS.

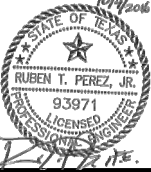
5. COMPLIANCE WITH ADA AND TAS FOR ACCESSIBLE ROUTES

- A. EVERY EFFORT HAS BEEN MADE BY THE ENGINEER TO COMPLY WITH THE AMERICANS WITH DISABILITIES ACT AND TEXAS ACCESSIBILITY STANDARDS. DURING CONSTRUCTION, THE CONTRACTOR IS ULTIMATELY RESPONSIBLE FOR CONSTRUCTING THE IMPROVEMENTS IN ACCORDANCE WITH THE STANDARDS INCLUDED IN THESE DOCUMENTS. CONFLICTS WITH THE PLANS AND/OR SPECIFICATIONS FOUND BY THE CONTRACTOR SHALL BE IMMEDIATELY REPORTED TO THE ENGINEER BEFORE PROCEEDING WITH CONSTRUCTION.
- B. THE MINIMUM STANDARDS OUTLINED BELOW SHALL BE ADHERED TO AT ALL TIMES.
- I. ALL SIDEWALKS SHALL BE A MINIMUM OF 4" THICK. SEE DETAIL SHEETS.
- II. THE FOLLOWING GENERAL TEXAS DEPARTMENT OF LICENSING AND REGULATION (TDLR) CRITERIA APPLY TO MANEUVERING SURFACES AT DOORS, ENTRIES, PORCHES, RAMP LANDINGS, PARKING AREAS, WALKWAYS AND PAVEMENT WHICH ARE PART OF A REQUIRED ACCESSIBLE ROUTE FOR ENTRY/EXIT.
- a. THE REQUIRED CLEAR FLOOR SPACE AREA AT ACCESSIBLE ENTRY/EXIST DOORS SHALL NOT HAVE A SLOPE THAT EXCEEDS 1:50 (2%) IN ANY DIRECTION.
- b. CROSS SLOPE FOR ACCESSIBLE WALKWAYS SHALL NOT EXCEED 1:50 (2%).
- c. RUNNING SLOPE FOR ACCESSIBLE WALKWAYS SHALL NOT EXCEED 1:20 (5%).
- d. PAVEMENT SURFACES THAT ARE PART OF A REQUIRED ACCESSIBLE ROUTE SHALL NOT EXCEED 1:50 (2%) CROSS SLOPE AND 1:20 (5%) RUNNING SLOPE.
- e. CHANGES IN GRADE IN ACCESSIBLE ROUTES GREATER THAN 1:20 (5%) REQUIRE A RAMP.
- f. ABRUPT CHANGES IN LEVEL IN EXCESS OF 1/4" ARE NOT PERMITTED.
- III. ACCESSIBLE ROUTE ELEVATIONS INDICATED ON THE GRADING PLAN ARE SCHEMATIC, AND ARE INTENDED TO COMPLY IN ALL RESPECTS WITH TDLR REQUIREMENTS. THE CONTRACTOR IS TO ADJUST GRADES AS NECESSARY TO FIT PARTICULAR CONDITIONS. NOTIFY ENGINEER AND REQUEST INSTRUCTION IF NON-COMPLIANT SITUATIONS ARE ENCOUNTERED OR ANTICIPATED.
- IV. CONCRETE SURFACE ALONG THE ACCESSIBLE PATHWAY SHALL RECEIVE A LIGHT BROOM FINISH UNLESS NOTED OTHERWISE.
- V. THE DETECTABLE WARNING PANELS MUST COMPLY WITH TEXAS ACCESSIBILITY STANDARDS 705 AND ADMINISTRATIVE RULES OF THE TDLR, 16 TAC CHAPTER 68, SECTION 68.102 AT A MINIMUM OF 24 INCHES IN DEPTH (IN THE DIRECTION OF PEDESTRIAN TRAVEL) AND EXTEND THE FULL WIDTH OF THE CURB. DETECTABLE WARNING PANELS MUST BE A TxDOT APPROVED PANEL MATERIAL. BRICK PAVERS OR METAL PANELS WILL NOT BE ALLOWED. THE PANELS SHALL BE RED IN COLOR. DETECTABLE WARNING PANEL MUST FOLLOW THE CURB LINE ON CURB RADIIUSES AND MAY BE NO MORE THAN 6 TO 10 INCHES FROM THE PROJECTED FACE OF CURB. CURVED PANELS OR CUT PANELS WILL BE REQUIRED.

BASE BID QUANTITIES

ITEM	DESCRIPTION	QUANTITY	UNIT	ITEM	DESCRIPTION	QUANTITY	UNIT
1.	NEW RAMP SECTION	1889	SF	9.	SAW CUT ASPHALT PAVEMENT (FULL DEPTH)	710	LF
2.	NEW LANDING SECTION	1755	SF	10.	PAVEMENT REPAIR	1293	SF
3.	NEW CONCRETE SHELTER PAD	8308	SF	11.	6" HEADER CURB	460	LF
4.	NEW SIDEWALK SECTION	5161	SF	12.	RELOCATE EXISTING BUS STOP SIGN	7	EA
5.	NEW CURB & GUTTER	542	LF	13.	BUS STOP CURB YELLOW STRIPING	1885	LF
6.	DEMOLITION OF EXITING SIDEWALK	10939	SF	14.	TRAFFIC CONTROL PLAN ALLOWANCE	1	LS
7.	DEMOLITION OF EXISTING CURB & GUTTER	526	LF	15.	BETTERMENT FUND ALLOWANCE	1	LS
8.	SAW CUT CONCRETE PAVEMENT (FULL DEPTH)	452	LF				

REV.	DESCRIPTION	APPR.	DATE



Drawn By	: A.N.
Checked By	: R.T.P.
Approved By	: R.T.P.
Project No.	: 5740102
Scale	: AS NOTED
Date	: 10/04/2016
Revision	: 0



Martinez, Guy & Maybik, Inc.
Civil / Structural Engineering & Surveying Services
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6000 S. Staples
Suite. 207
Corpus Christi
Tx. 78413
P: 361-814-3070
Fax: 888-653-5510



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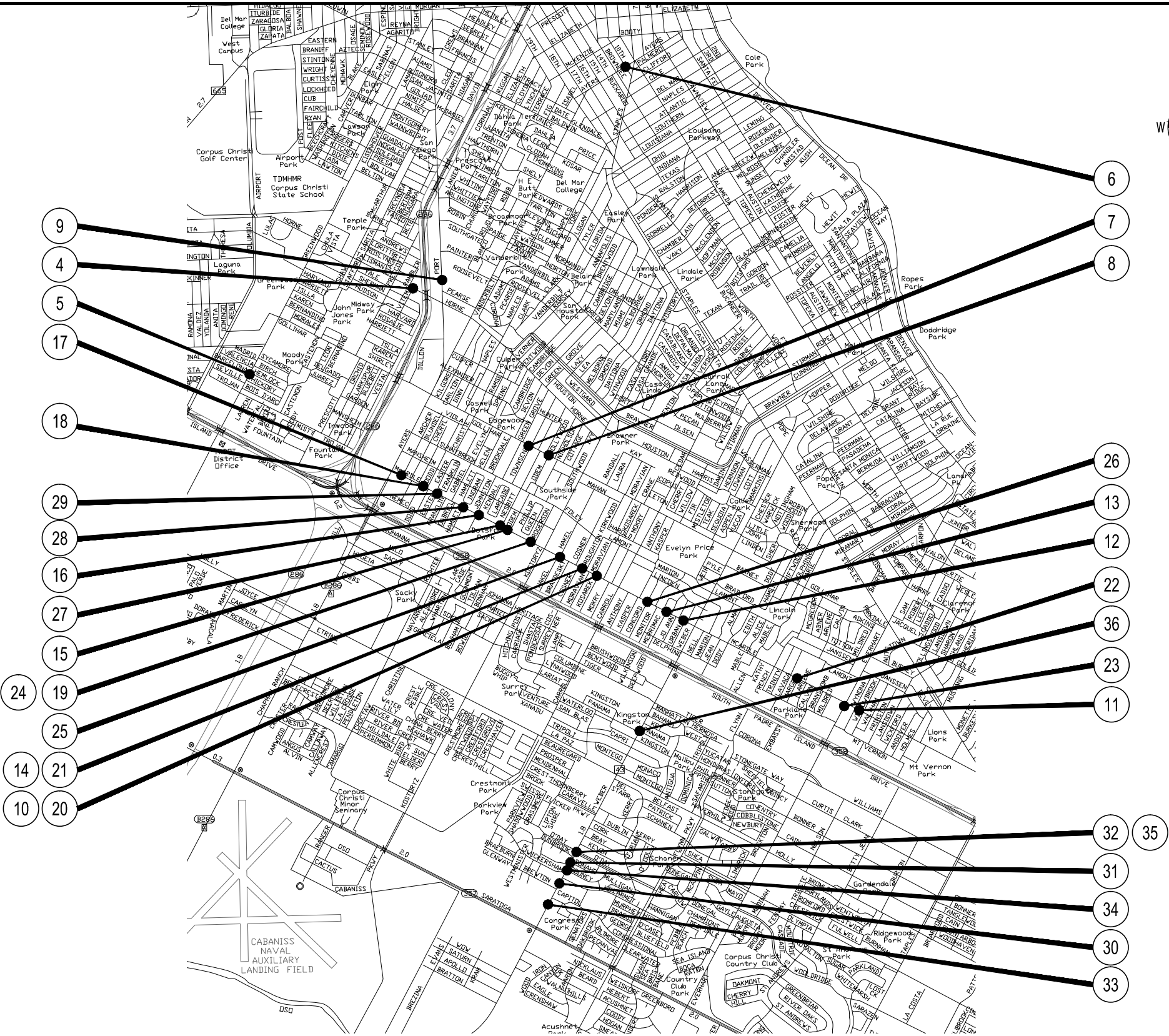
ADA BUS STOP IMPROVEMENTS
PHASE VI
GENERAL NOTES
REGIONAL TRANSPORTATION AUTHORITY
CORPUS CHRISTI, TEXAS

Dwg. File:

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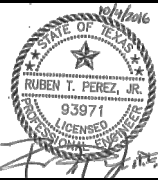
SHEET No.	BUS STOP ID & STREET NAME		
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31	STOP ID	972	— FM 43 (WEBER) AT ROONEY
32	STOP ID	971	— FM 43 (WEBER) AT O'DAY
33	STOP ID	974	— FM 43 (WEBER) AT SH 357 (SARATOGA)
34	STOP ID	1027	— FM 43 (WEBER) AT ROONEY
35	STOP ID	1028	— FM 43 (WEBER) AT O'DAY
36	STOP ID	1035	— FM 43 (WEBER) AT PANAMA



CORPUS CHRISTI, TEXAS
N.T.S.

NOTE: ## REPRESENTS SHEET NUMBER

REV.	DESCRIPTION	APPR.	DATE



Drawn By	: V.M.
Checked By	: R.T.P.
Approved By	: R.T.P.
Project No.	: 5740102
Scale	: AS NOTED
Date	: 10/04/2016
Revision	: 0



6000 S. Staples
Suite, 207
Corpus Christi
Tx. 78413
P: 361-814-3070
Fax: 888-653-5510

Martinez, Guy & Maybik, Inc.

Civil / Structural Engineering & Surveying Services
TX. Bd. P.E. Firm Reg. No. F-1415



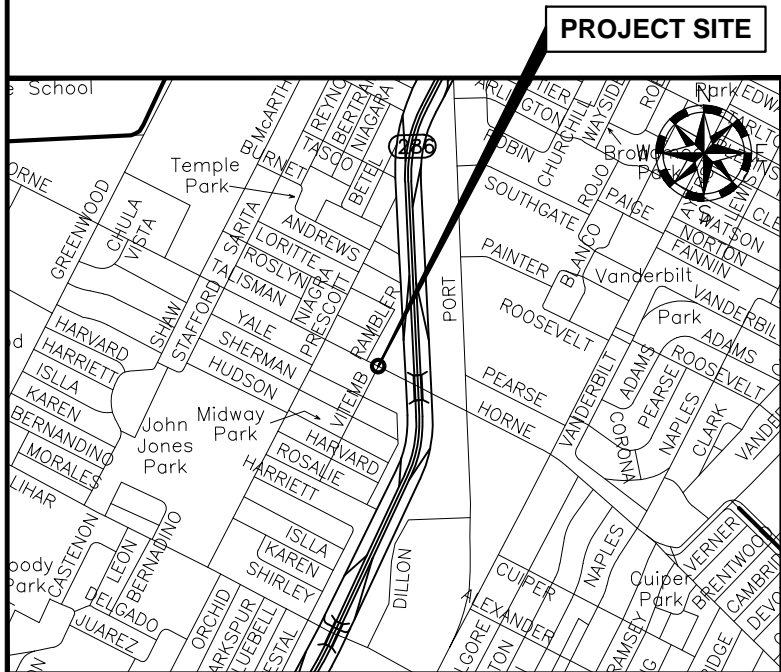
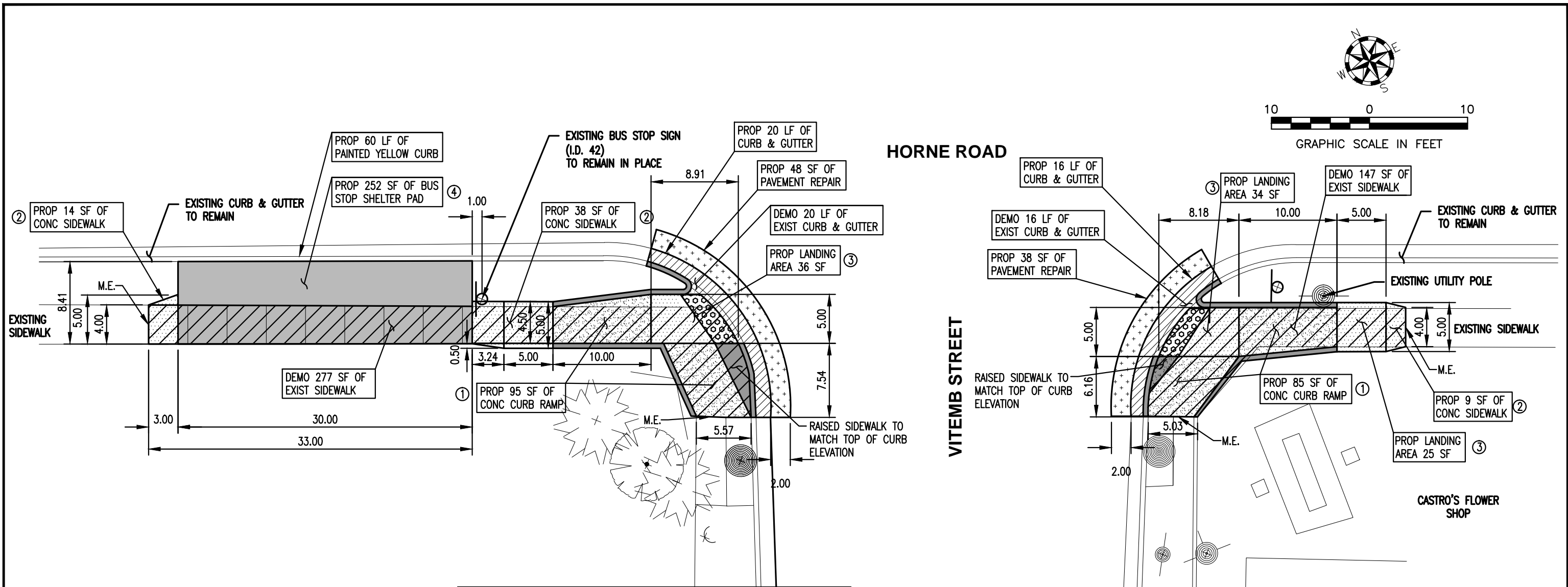
CORPUS CHRISTI REGIONAL
TRANSPORTATION AUTHORITY

**ADA BUS STOP IMPROVEMENTS
PHASE VI**

GENERAL LOCATION MAP

REGIONAL TRANSPORTATION AUTHORITY

CORPUS CHRISTI, TEXAS



NEW CONCRETE SHELTER PAD	252	SF
NEW CONCRETE SIDEWALK	61	SF
NEW CURB & GUTTER	36	LF
NEW RAMP SECTION	180	SF
NEW LANDING SECTION	95	SF
DEMOLITION OF EXISTING SIDEWALK	424	SF
DEMOLITION OF EXISTING CURB & GUTTER	36	LF
SAW CUT CONCRETE PAVEMENT (FULL DEPTH)	19	LF
SAW CUT ASPHALT PAVEMENT (FULL DEPTH)	48	LF
PAVEMENT REPAIR	86	SF
BUS STOP YELLOW STRIPING	60	LF

NOTES:

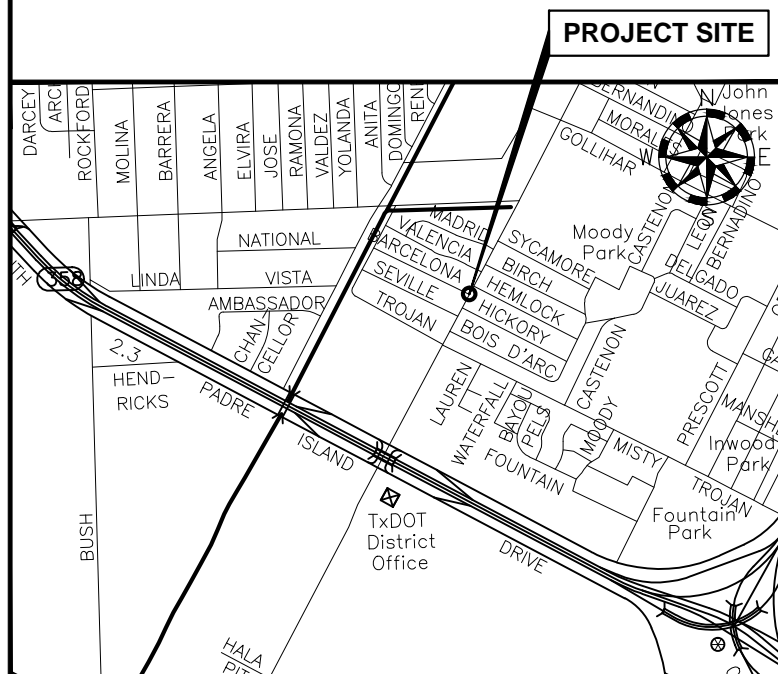
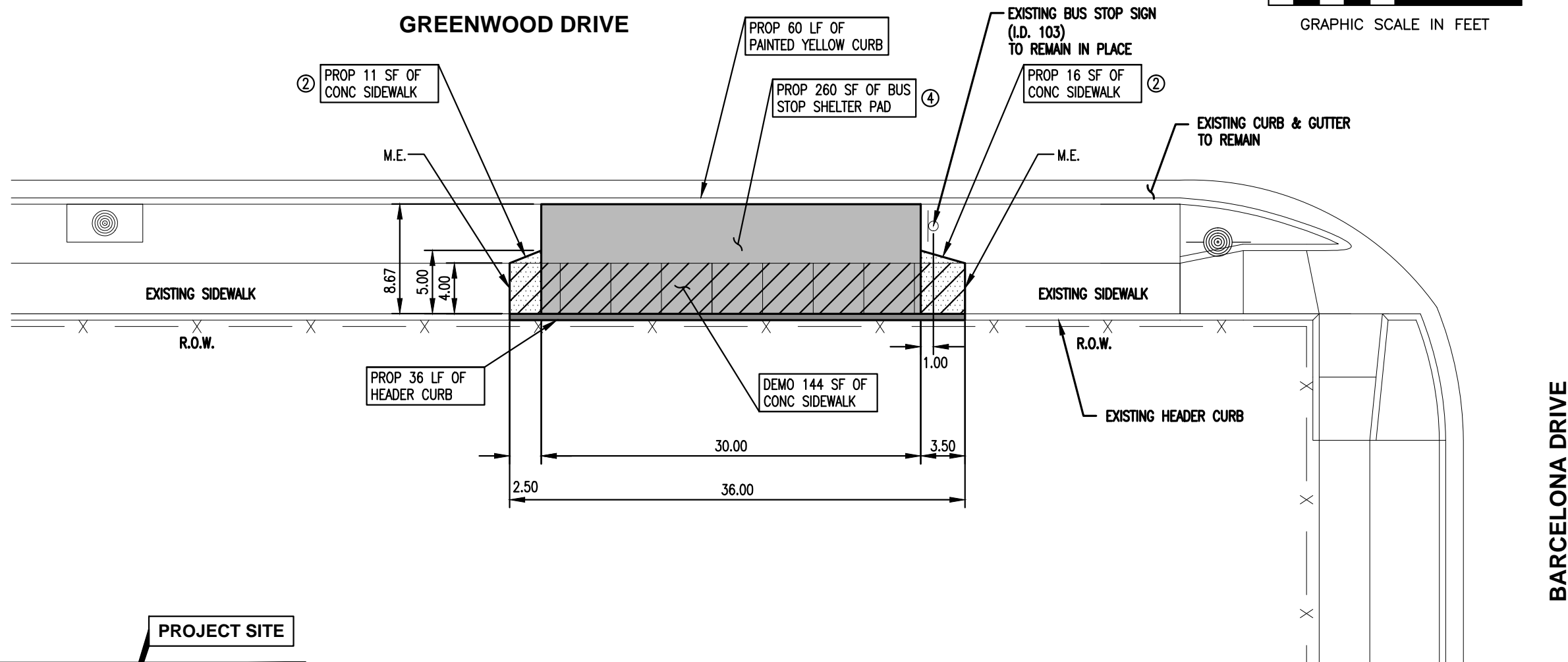
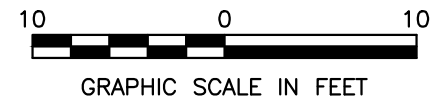
M.E. = MATCH EXISTING ELEVATION

CROSS SLOPES FOR ALL CURB RAMPS, LANDINGS, SIDEWALKS, AND BUS STOP SHELTER PADS SHALL BE A MAXIMUM OF 2.0%

KEY NOTES:

- ① CONCRETE CURB RAMPS = 8.3% MAX SLOPE
- ② CONCRETE SIDEWALKS = 5.0% MAX SLOPE
- ③ CONCRETE LANDINGS = 2.0% MAX SLOPE ALL DIRECTIONS
- ④ CONCRETE BUS STOP SHELTER PAD = 2.0% MAX SLOPE ALL DIRECTIONS

BUS HEADING SOUTHEAST
BUS STOP SIGN ID 42



NEW CONCRETE SHELTER PAD	260	SF
NEW SIDEWALK SECTION	27	SF
SAW CUT CONCRETE PAVEMENT (FULL DEPTH)	8	LF
DEMOLITION OF EXISTING SIDEWALK	144	SF
6" HEADER CURB	36	LF
BUS STOP CURB YELLOW STRIPING	60	LF

NOTES:

M.E. = MATCH EXISTING ELEVATION

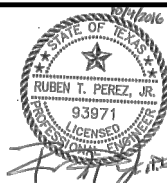
CROSS SLOPES FOR ALL CURB RAMPS, LANDINGS, SIDEWALKS, AND BUS STOP SHELTER PADS SHALL BE A MAXIMUM OF 2.0%

KEY NOTES:

- ① CONCRETE CURB RAMPS = 8.3% MAX SLOPE
- ② CONCRETE SIDEWALKS = 5.0% MAX SLOPE
- ③ CONCRETE LANDINGS = 2.0% MAX SLOPE ALL DIRECTIONS
- ④ CONCRETE BUS STOP = 2.0% MAX SLOPE ALL DIRECTIONS SHELTER PAD

BUS HEADING SOUTHWEST
BUS STOP SIGN ID 103

REV.	DESCRIPTION	APPR.	DATE

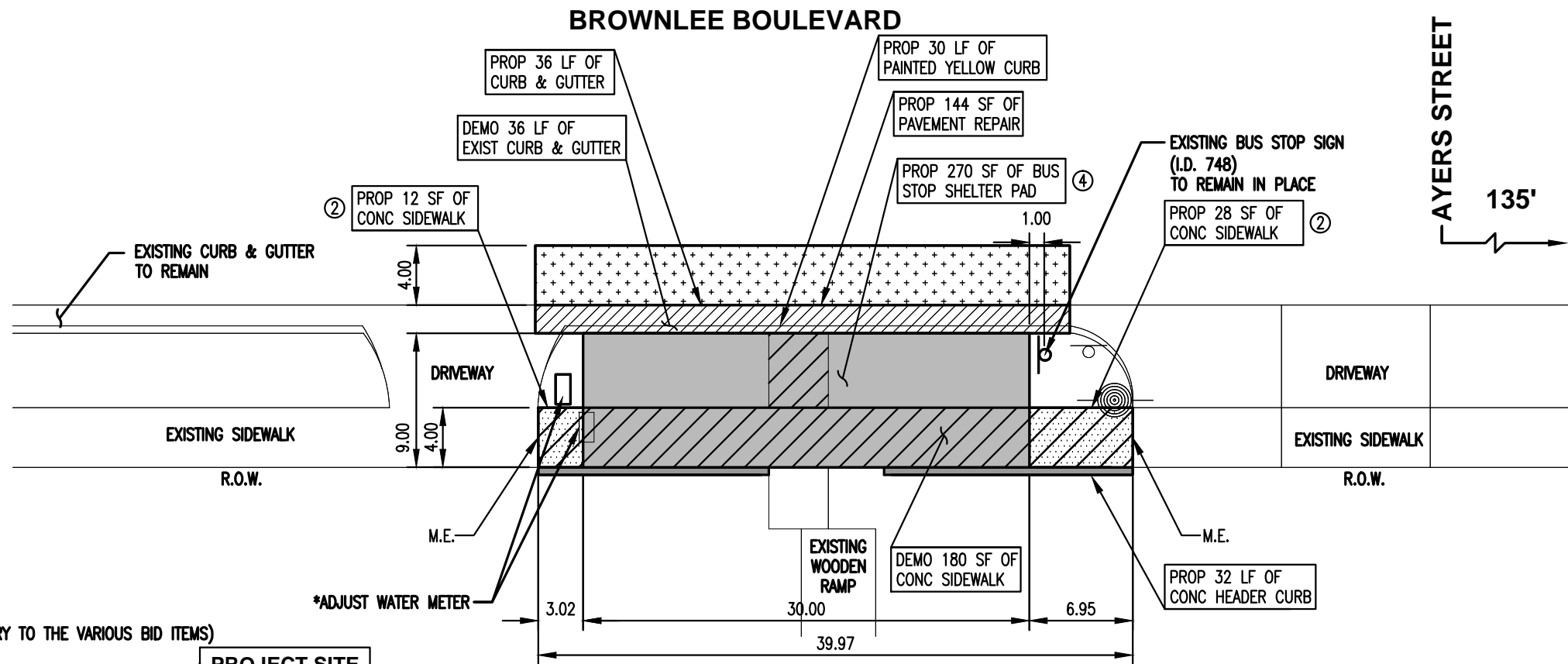
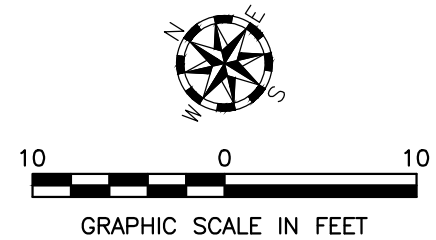


Drawn By	: V.M.
Checked By	: R.T.P.
Approved By	: R.T.P.
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Scale	: AS NOTED
Date	: 10/04/2016
Revision	: 0



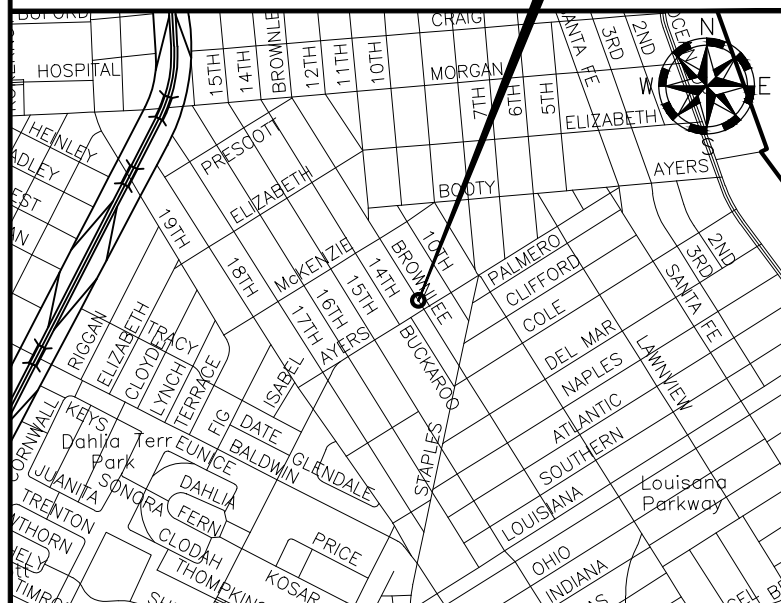
CORPUS CHRISTI REGIONAL
TRANSPORTATION AUTHORITY

ADA BUS STOP IMPROVEMENTS
PHASE VI
BUS STOP 103 - GREENWOOD AT BARCELONA
REGIONAL TRANSPORTATION AUTHORITY
CORPUS CHRISTI, TEXAS



*(SUBSIDIARY TO THE VARIOUS BID ITEMS)

PROJECT SITE



NEW CONCRETE SHELTER PAD	270	SF
NEW SIDEWALK SECTION	40	SF
NEW CURB & GUTTER	36	LF
DEMOLITION OF EXISTING SIDEWALK	180	SF
DEMOLITION OF CURB & GUTTER	36	SF
SAW CUT CONCRETE PAVEMENT (FULL DEPTH)	12	LF
SAW CUT ASPHALT PAVEMENT (FULL DEPTH)	44	LF
PAVEMENT REPAIR	144	SF
6" HEADER CURB	32	SF
BUS STOP CURB YELLOW STRIPING	30	LF

NOTES:

M.E. = MATCH EXISTING ELEVATION

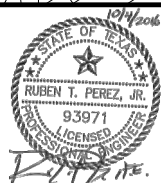
CROSS SLOPES FOR ALL CURB RAMPS, LANDINGS, SIDEWALKS, AND BUS STOP SHELTER PADS SHALL BE A MAXIMUM OF 2.0%

KEY NOTES:

- ① CONCRETE CURB RAMPS = 8.3% MAX SLOPE
- ② CONCRETE SIDEWALKS = 5.0% MAX SLOPE
- ③ CONCRETE LANDINGS = 2.0% MAX SLOPE ALL DIRECTIONS
- ④ CONCRETE BUS STOP SHELTER PAD = 2.0% MAX SLOPE ALL DIRECTIONS

BUS HEADING SOUTHEAST
BUS STOP SIGN ID 748

REV.	DESCRIPTION	APPR.	DATE

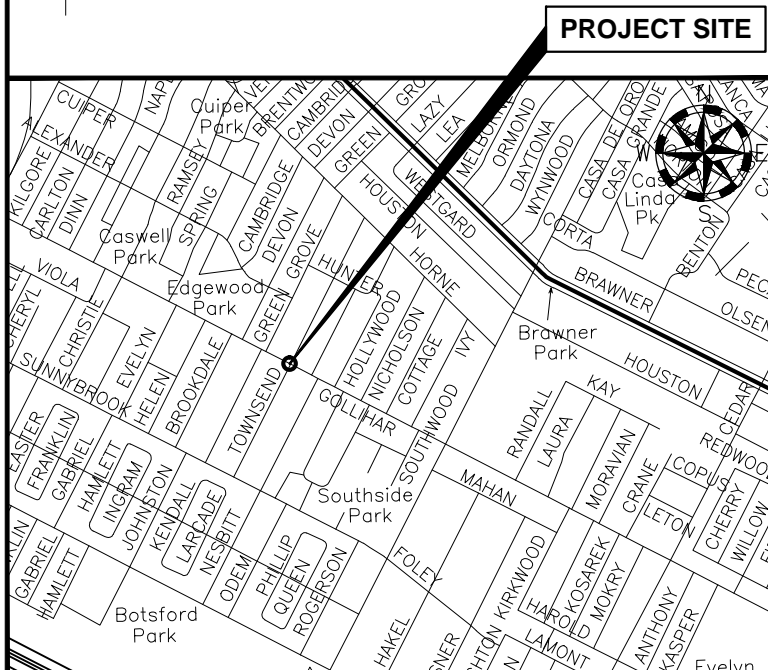
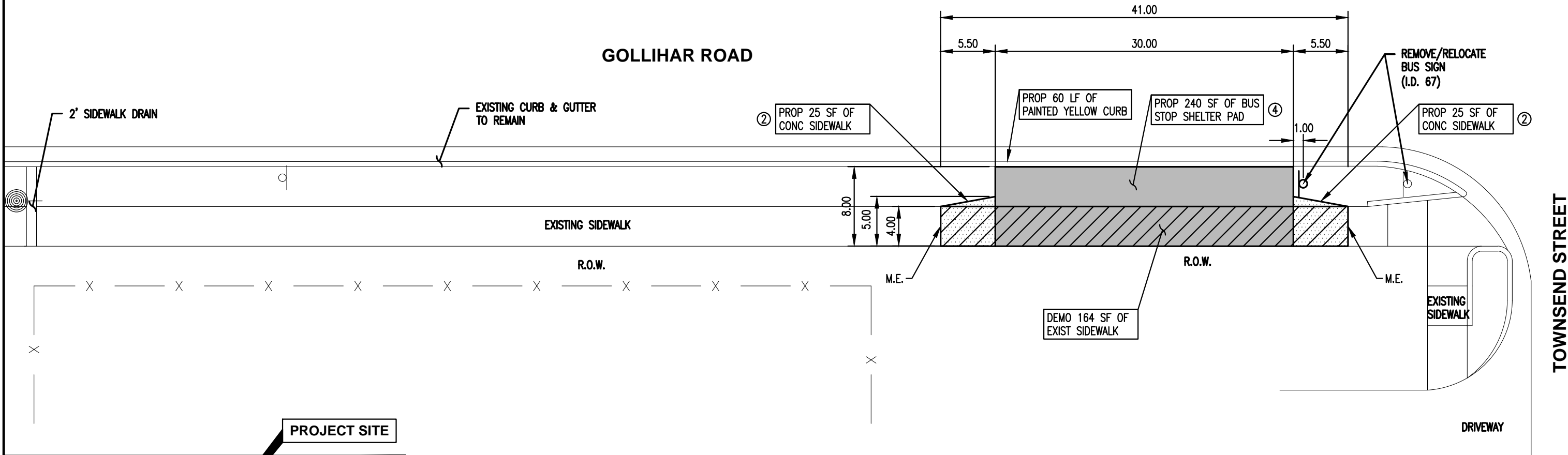
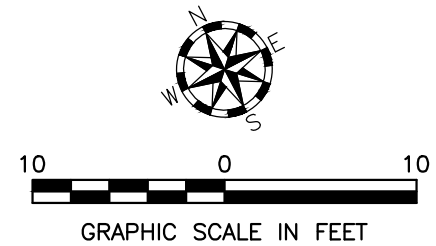


Drawn By	: V.M.
Checked By	: R.T.P.
Approved By	: R.T.P.
Project No.	: 5740102
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Date	: 10/04/2016
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mgm
6000 S. Staples
Suite 207
Corpus Christi
TX 78413
P: 361-814-3070
Fax: 888-653-5510
Martinez, Guy & Maybik, Inc.
Civil / Structural Engineering & Surveying Services
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B
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TRANSPORTATION AUTHORITY

**ADA BUS STOP IMPROVEMENTS
PHASE VI**
BUS STOP 748 - AYERS AT BROWNLEE
REGIONAL TRANSPORTATION AUTHORITY
CORPUS CHRISTI, TEXAS



NEW CONCRETE SHELTER PAD	240	SF
NEW SIDEWALK SECTION	50	SF
DEMOLITION OF EXISTING SIDEWALK	164	SF
SAW CUT CONCRETE PAVEMENT (FULL DEPTH)	8	LF
RELOCATE EXISTING BUS STOP SIGN	1	EA
BUS STOP CURB YELLOW STRIPING	60	LF

NOTES:

M.E. = MATCH EXISTING ELEVATION

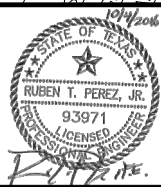
CROSS SLOPES FOR ALL CURB RAMPS, LANDINGS, SIDEWALKS, AND BUS STOP SHELTER PADS SHALL BE A MAXIMUM OF 2.0%

KEY NOTES:

- ① CONCRETE CURB RAMPS = 8.3% MAX SLOPE
- ② CONCRETE SIDEWALKS = 5.0% MAX SLOPE
- ③ CONCRETE LANDINGS = 2.0% MAX SLOPE ALL DIRECTIONS
- ④ CONCRETE BUS STOP = 2.0% MAX SLOPE ALL DIRECTIONS SHELTER PAD

BUS HEADING SOUTHEAST
BUS STOP SIGN ID 67

REV.	DESCRIPTION	APPR.	DATE



Drawn By	: V.M.
Checked By	: R.T.P.
Approved By	: R.T.P.
Project No.	: 5740102
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Date	: 10/04/2016
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6000 S. Staples
Suite 207
Corpus Christi
Tx. 78413
P: 361-814-3070
Fax: 888-653-5510

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CORPUS CHRISTI REGIONAL
TRANSPORTATION AUTHORITY

ADA BUS STOP IMPROVEMENTS
PHASE VI

BUS STOP 67 - GOLLIHAR AT TOWNSEND

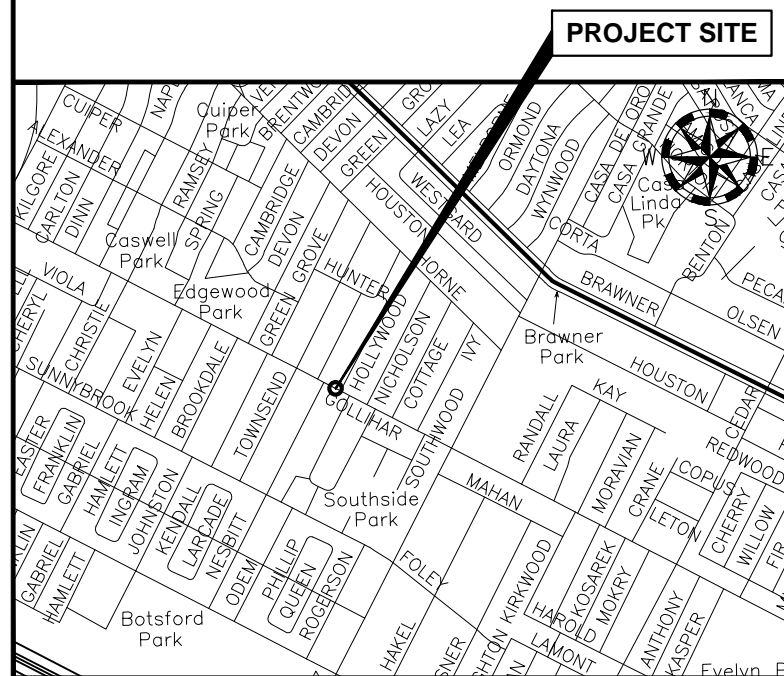
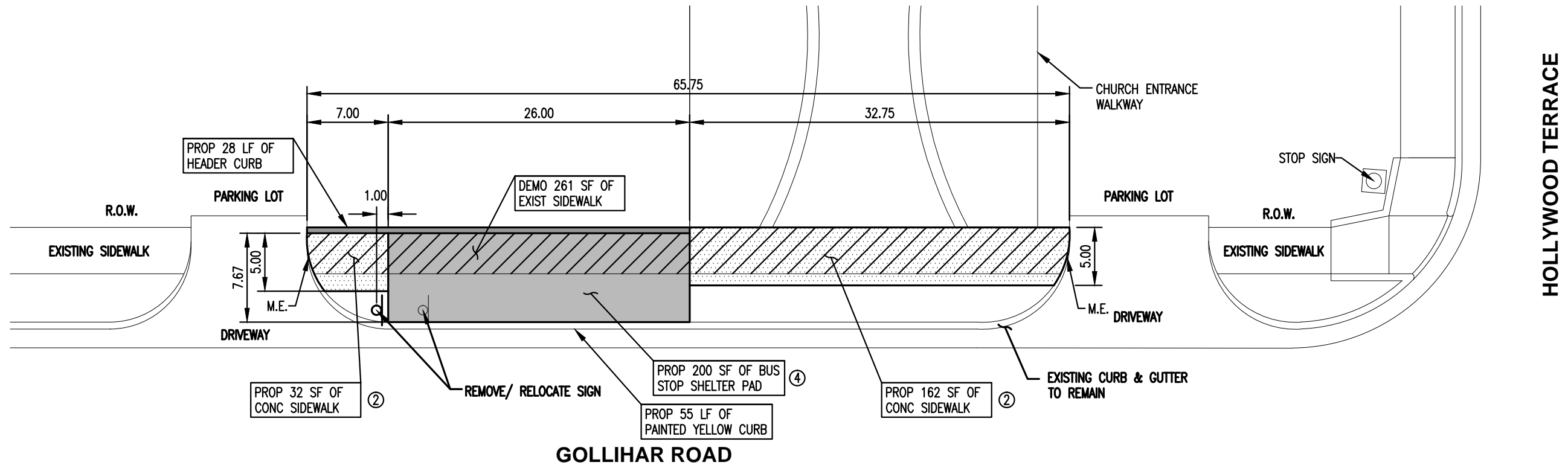
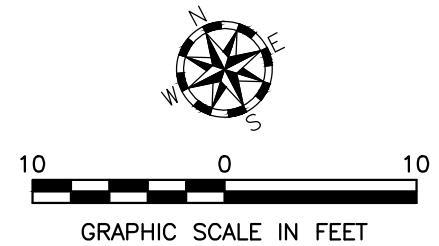
REGIONAL TRANSPORTATION AUTHORITY

CORPUS CHRISTI, TEXAS

Dwg. File:

7

Sheet 7 of 44



NEW CONCRETE SHELTER PAD	200	SF
NEW SIDEWALK SECTION	194	SF
DEMOLITION OF EXISTING SIDEWALK	261	SF
SAW CUT CONCRETE PAVEMENT (FULL DEPTH)	8	LF
6" HEADER CURB	28	LF
BUS STOP CURB YELLOW STRIPING	55	LF

NOTES:

M.E. = MATCH EXISTING ELEVATION

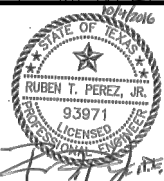
CROSS SLOPES FOR ALL CURB RAMPS, LANDINGS, SIDEWALKS, AND BUS STOP SHELTER PADS SHALL BE A MAXIMUM OF 2.0%

KEY NOTES:

- ① CONCRETE CURB RAMPS = 8.3% MAX SLOPE
- ② CONCRETE SIDEWALKS = 5.0% MAX SLOPE
- ③ CONCRETE LANDINGS = 2.0% MAX SLOPE ALL DIRECTIONS
- ④ CONCRETE BUS STOP = 2.0% MAX SLOPE ALL DIRECTIONS SHELTER PAD

BUS HEADING NORTHWEST
BUS STOP SIGN ID 76

REV.	DESCRIPTION	APPR.	DATE



Drawn By	: V.M.
Checked By	: R.T.P.
Approved By	: R.T.P.
Project No.	: 5740102
Scale	: AS NOTED
Date	: 10/04/2016
Revision	: 0



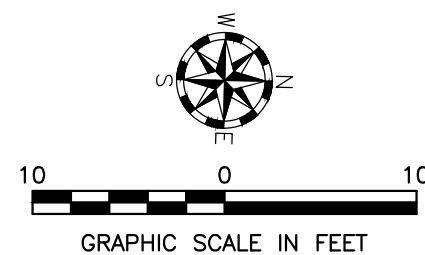
6000 S. Staples
Suite 207
Corpus Christi
TX 78413
P: 361-814-3070
Fax: 888-653-5510

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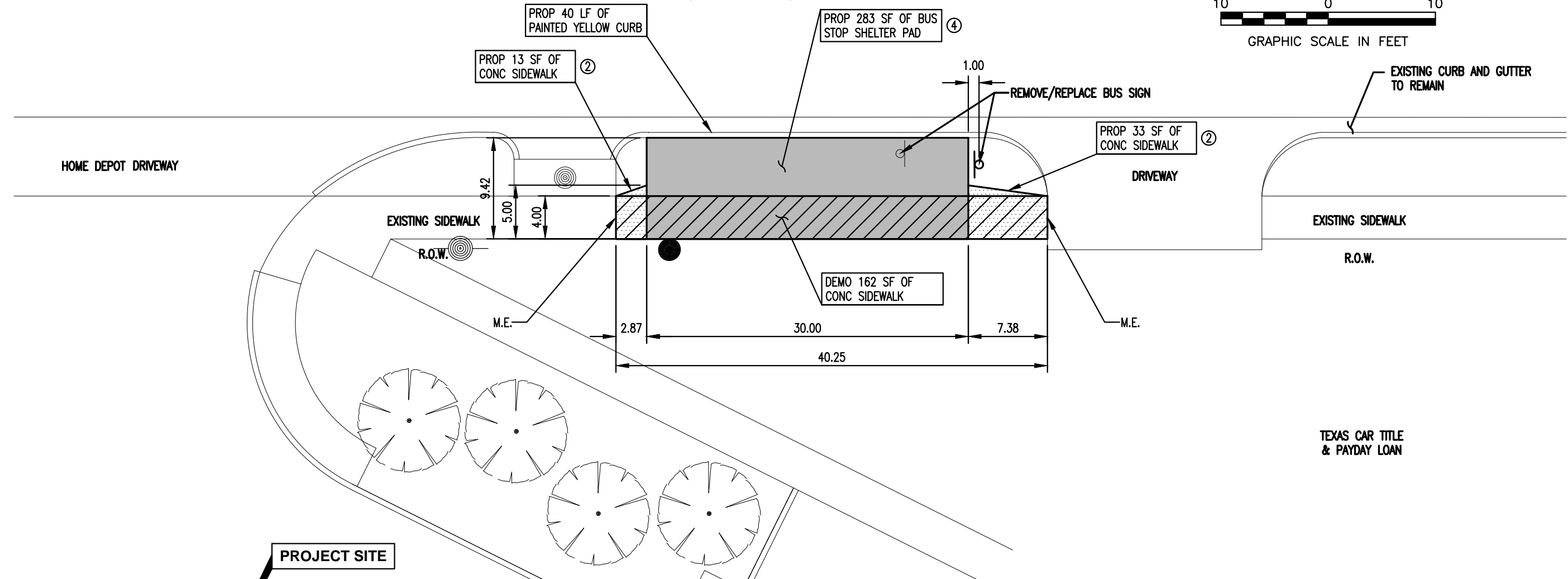


CORPUS CHRISTI REGIONAL
TRANSPORTATION AUTHORITY

**ADA BUS STOP IMPROVEMENTS
PHASE VI**
BUS STOP 76 - GOLLIHAR AT HOLLYWOOD
REGIONAL TRANSPORTATION AUTHORITY
CORPUS CHRISTI, TEXAS

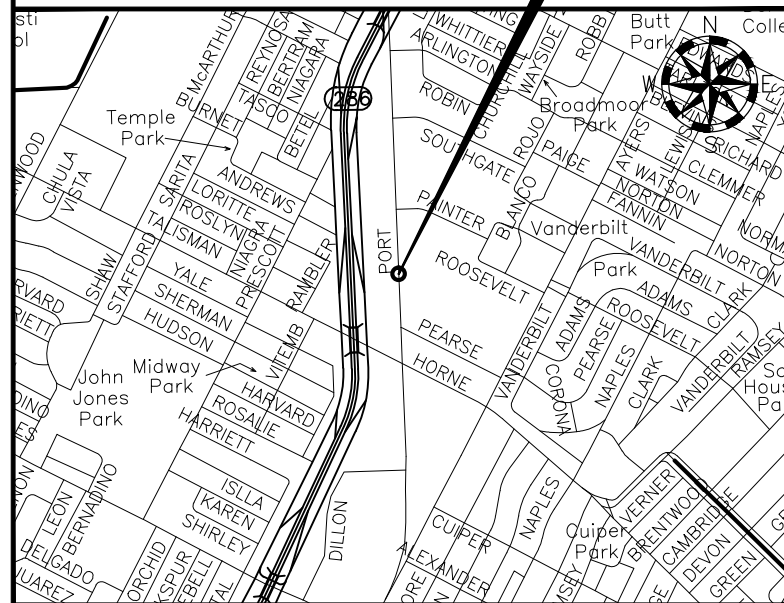


PORT AVENUE



TEXAS CAR TITLE
& PAYDAY LOAN

PROJECT SITE



NEW CONCRETE SHELTER PAD	283	SF
NEW SIDEWALK SECTION	46	SF
SAW CUT CONCRETE PAVEMENT (FULL DEPTH)	8	LF
DEMOLITION OF EXISTING SIDEWALK	162	SF
RELOCATE EXISTING BUS STOP SIGN	1	EA
BUS STOP CURB YELLOW STRIPING	40	LF

NOTES:

M.E. = MATCH EXISTING ELEVATION

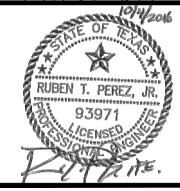
CROSS SLOPES FOR ALL CURB RAMPS,
LANDINGS, SIDEWALKS, AND BUS STOP SHELTER
PADS SHALL BE A MAXIMUM OF 2.0%

KEY NOTES:

- ① CONCRETE CURB RAMPS = 8.3% MAX SLOPE
- ② CONCRETE SIDEWALKS = 5.0% MAX SLOPE
- ③ CONCRETE LANDINGS = 2.0% MAX SLOPE ALL DIRECTIONS
- ④ CONCRETE BUS STOP = 2.0% MAX SLOPE ALL DIRECTIONS
SHELTER PAD

BUS HEADING NORTH
BUS STOP SIGN ID 9027

REV.	DESCRIPTION	APPR.	DATE




Drawn By	: V.M.
Checked By	: R.T.P.
Approved By	: R.T.P.
Project No.	: 5740102
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Date	: 10/04/2016
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6000 S. Staples
Suite 207
Corpus Christi
Tx. 78413
P: 361-814-3070
Fax: 888-653-5510

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ADA BUS STOP IMPROVEMENTS
PHASE VI

BUS STOP 9027 - PORT AT HOME DEPOT

REGIONAL TRANSPORTATION AUTHORITY

CORPUS CHRISTI, TEXAS

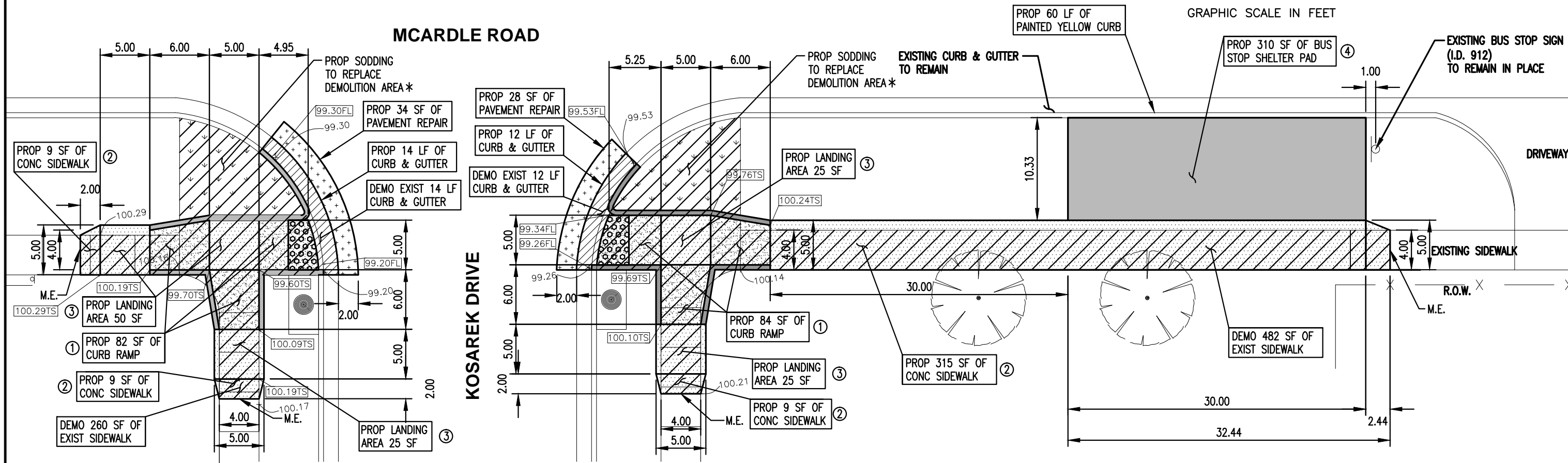
Dwg. File:

9

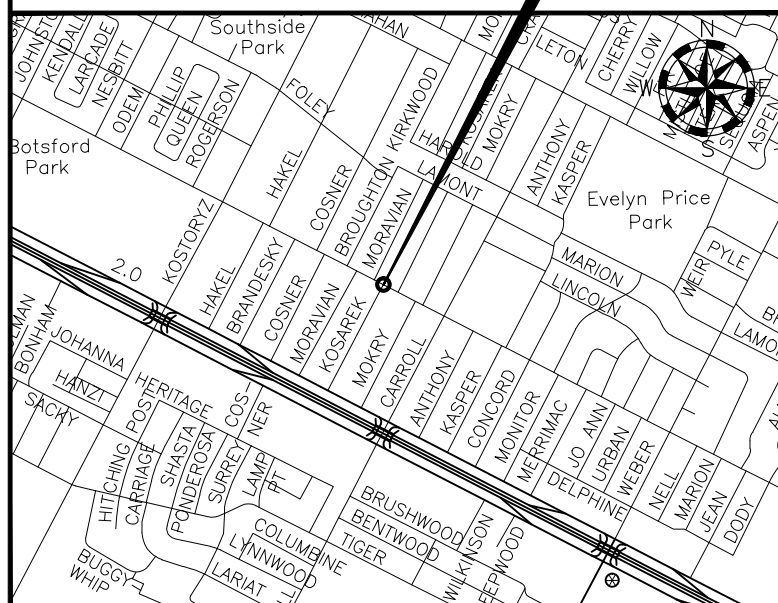
Sheet 9 of 44



GRAPHIC SCALE IN FEET



PROJECT SITE



NEW RAMP SECTION	166	SF
NEW LANDING SECTION	125	SF
NEW CONCRETE SHELTER PAD	310	SF
NEW SIDEWALK SECTION	342	SF
NEW CURB & GUTTER	26	SF
DEMOLITION OF EXISTING SIDEWALK	742	LF
DEMOLITION OF EXISTING CURB & GUTTER	26	SF
SAW CUT CONCRETE PAVEMENT (FULL DEPTH)	16	LF
SAW CUT ASPHALT PAVEMENT (FULL DEPTH)	38	LF
PAVEMENT REPAIR	62	SF
BUS STOP CURB YELLOW STRIPING	60	LF

* SODDING SHALL BE CONSIDERED
SUBSIDIARY TO THE VARIOUS BID ITEMS

NOTES:

M.E. = MATCH EXISTING ELEVATION

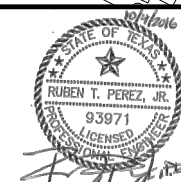
CROSS SLOPES FOR ALL CURB RAMPS,
LANDINGS, SIDEWALKS, AND BUS STOP SHELTER
PADS SHALL BE A MAXIMUM OF 2.0%

KEY NOTES:

- ① CONCRETE CURB RAMPS = 8.3% MAX SLOPE
- ② CONCRETE SIDEWALKS = 5.0% MAX SLOPE
- ③ CONCRETE LANDINGS = 2.0% MAX SLOPE ALL DIRECTIONS
- ④ CONCRETE BUS STOP = 2.0% MAX SLOPE ALL DIRECTIONS
SHELTER PAD

BUS HEADING NORTHWEST
BUS STOP SIGN ID 912

REV.	DESCRIPTION	APPR.	DATE



Drawn By	: V.M.
Checked By	: R.T.P.
Approved By	: R.T.P.
Project No.	: 5740102
Scale	: AS NOTED
Date	: 10/04/2016
Revision	: 0



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Suite 207
Corpus Christi
TX 78413
P: 361-814-3070
Fax: 888-653-5510
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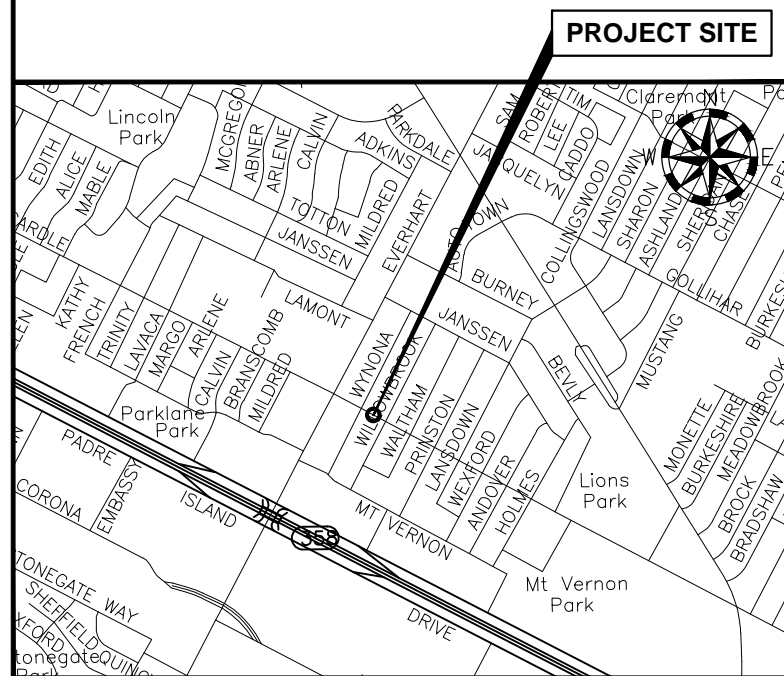
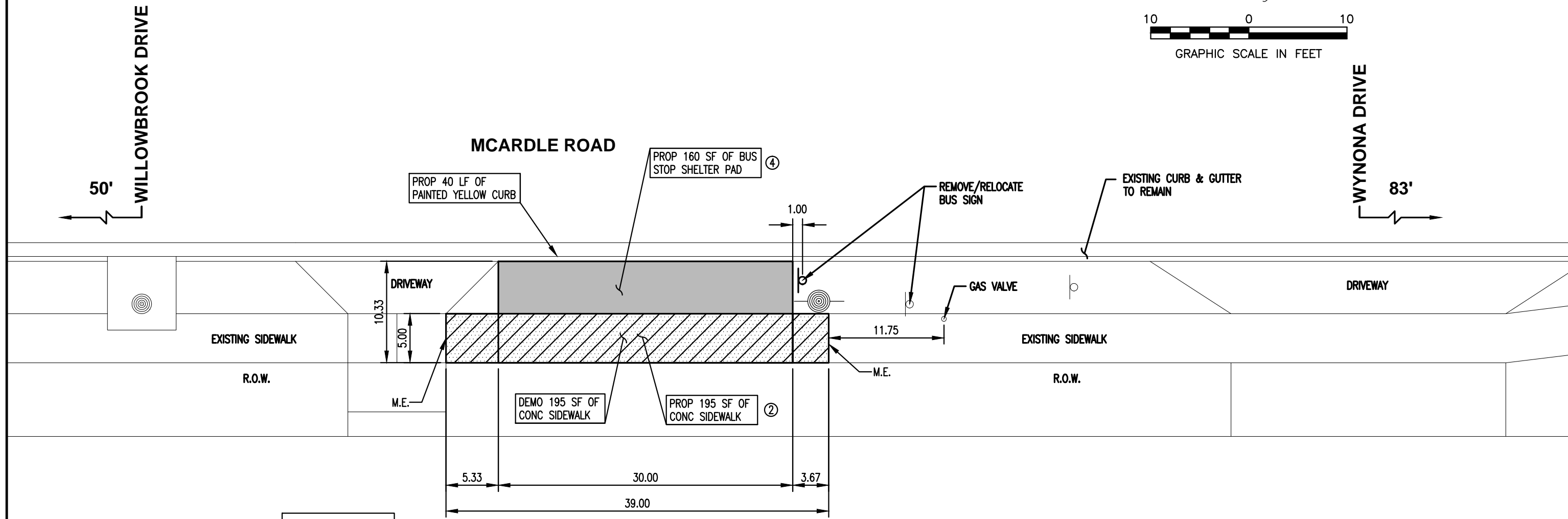
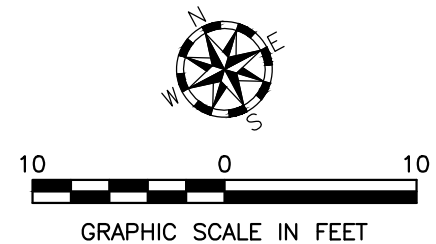
CORPUS CHRISTI REGIONAL
TRANSPORTATION AUTHORITY

**ADA BUS STOP IMPROVEMENTS
PHASE VI**
BUS STOP 912 - MCARDLE AT KOSAREK
REGIONAL TRANSPORTATION AUTHORITY
CORPUS CHRISTI, TEXAS

Dwg. File:

10

Sheet 10 of 44



NEW CONCRETE SHELTER PAD	160	SF
NEW SIDEWALK SECTION	195	SF
SAW CUT CONCRETE PAVEMENT (FULL DEPTH)	10	LF
DEMOLITION OF EXISTING SIDEWALK	195	SF
RELOCATE EXISTING BUS STOP SIGN	1	EA
BUS STOP CURB YELLOW STRIPING	40	LF

NOTES:

M.E. = MATCH EXISTING ELEVATION

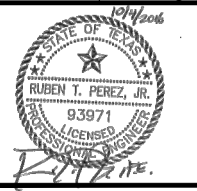
CROSS SLOPES FOR ALL CURB RAMPS, LANDINGS, SIDEWALKS, AND BUS STOP SHELTER PADS SHALL BE A MAXIMUM OF 2.0%

KEY NOTES:

- ① CONCRETE CURB RAMPS = 8.3% MAX SLOPE
- ② CONCRETE SIDEWALKS = 5.0% MAX SLOPE
- ③ CONCRETE LANDINGS = 2.0% MAX SLOPE ALL DIRECTIONS
- ④ CONCRETE BUS STOP = 2.0% MAX SLOPE ALL DIRECTIONS SHELTER PAD

BUS HEADING SOUTHEAST
BUS STOP SIGN ID 845

REV.	DESCRIPTION	APPR.	DATE




Drawn By	: V.M.
Checked By	: R.T.P.
Approved By	: R.T.P.
Project No.	: 5740102
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6000 S. Staples
Suite 207
Corpus Christi
Tx 78413
P: 361-814-3070
Fax: 888-653-5510

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Civil / Structural Engineering & Surveying Services
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**CORPUS CHRISTI REGIONAL
TRANSPORTATION AUTHORITY**



**ADA BUS STOP IMPROVEMENTS
PHASE VI**

BUS STOP 845 - MCARDLE AT WYNONA

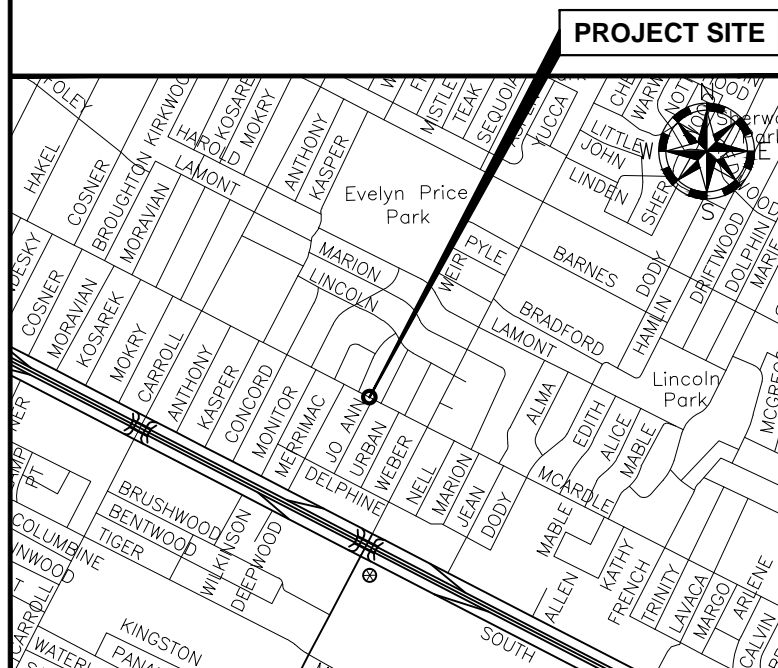
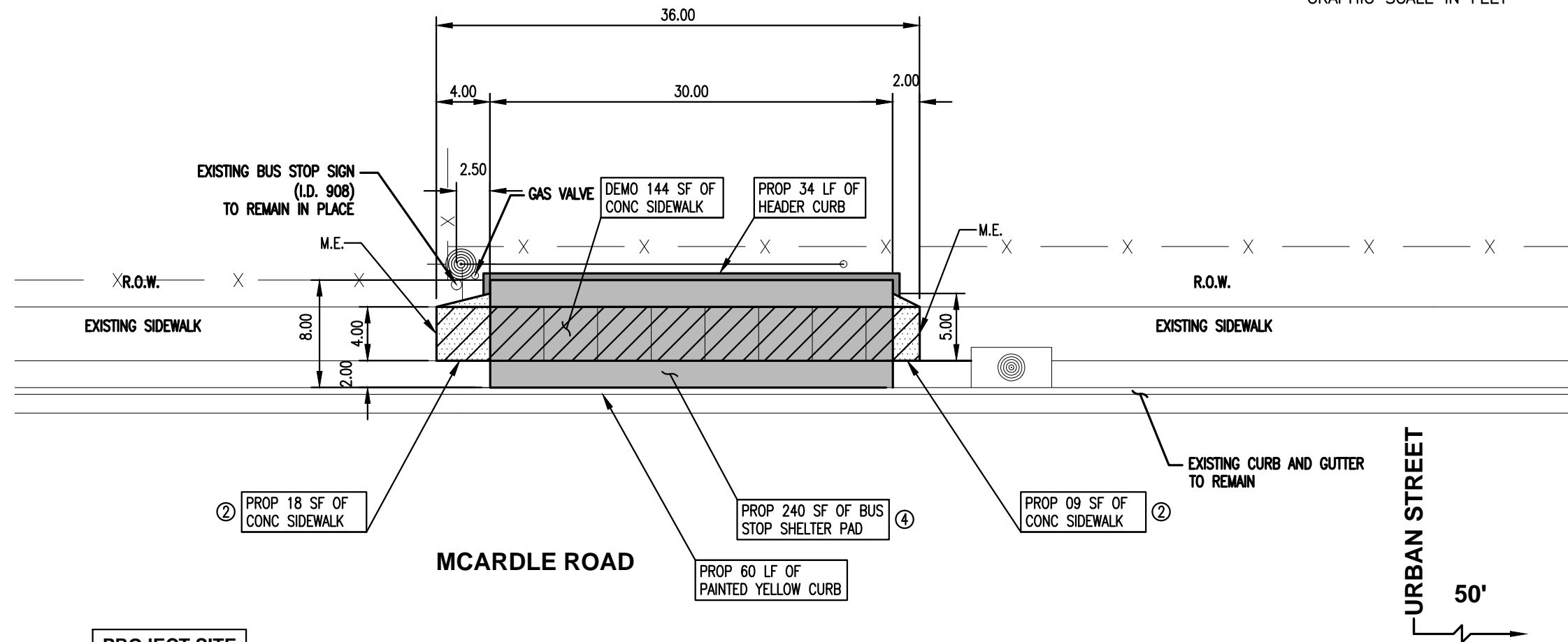
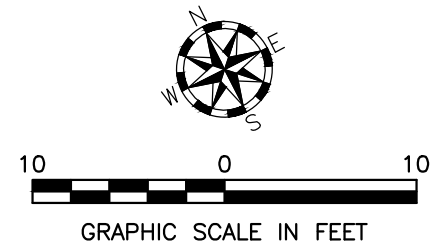
REGIONAL TRANSPORTATION AUTHORITY

CORPUS CHRISTI, TEXAS

Dwg. File:

11

Sheet 11 of 44



NEW CONCRETE SHELTER PAD	240	SF
NEW SIDEWALK SECTION	27	SF
6" HEADER CURB	34	LF
SAW CUT CONCRETE PAVEMENT (FULL DEPTH)	8	LF
DEMOLITION OF EXISTING SIDEWALK	144	SF
BUS STOP CURB YELLOW STRIPING	60	LF

NOTES:

M.E. = MATCH EXISTING ELEVATION

CROSS SLOPES FOR ALL CURB RAMPS, LANDINGS, SIDEWALKS, AND BUS STOP SHELTER PADS SHALL BE A MAXIMUM OF 2.0%

KEY NOTES:

- ① CONCRETE CURB RAMPS = 8.3% MAX SLOPE
- ② CONCRETE SIDEWALKS = 5.0% MAX SLOPE
- ③ CONCRETE LANDINGS = 2.0% MAX SLOPE ALL DIRECTIONS
- ④ CONCRETE BUS STOP = 2.0% MAX SLOPE ALL DIRECTIONS SHELTER PAD

BUS HEADING SOUTHEAST
BUS STOP SIGN ID 908

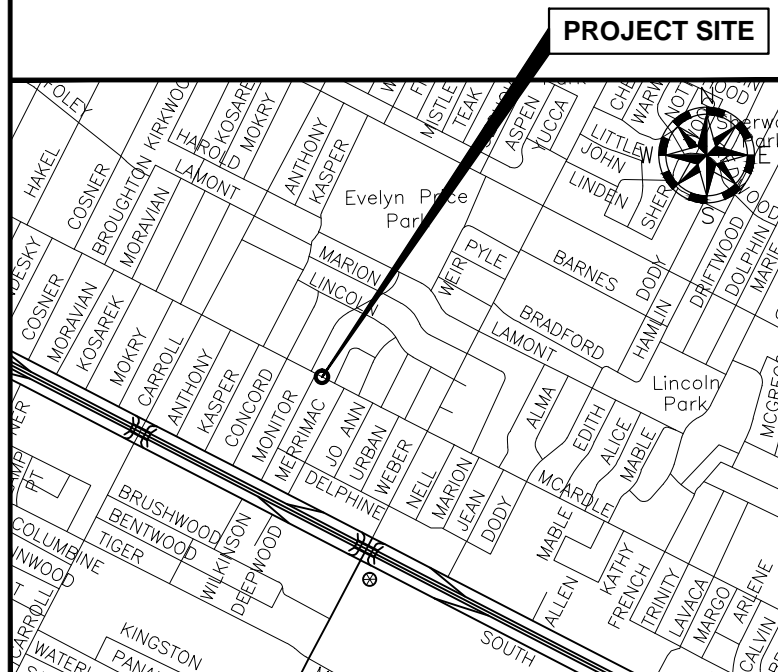
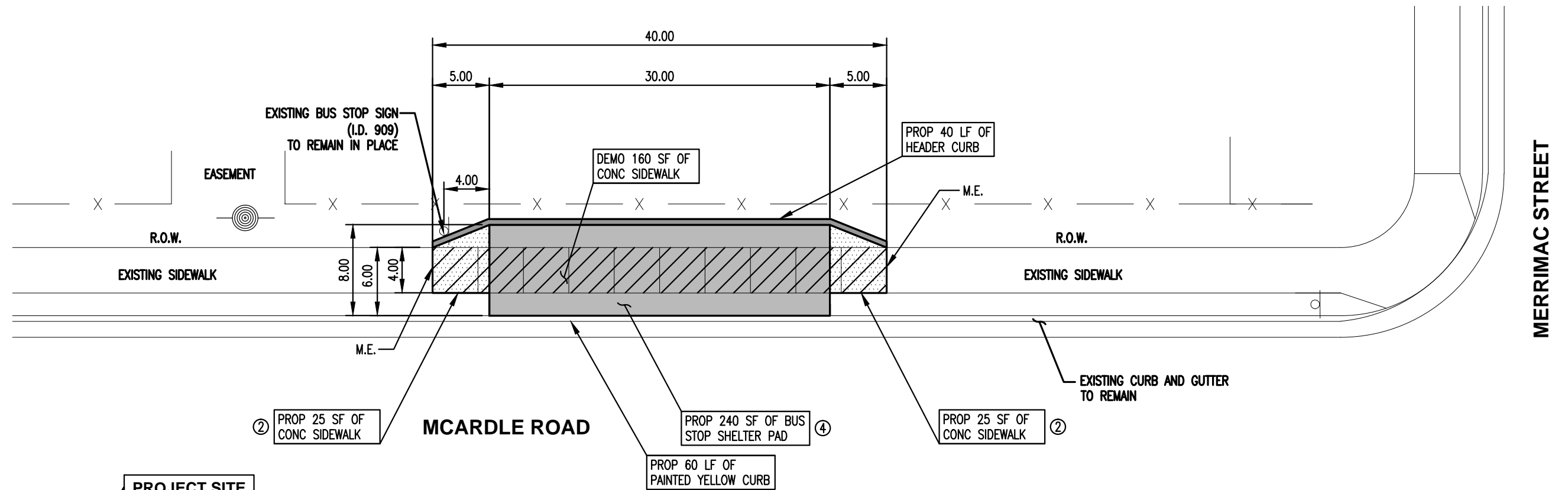
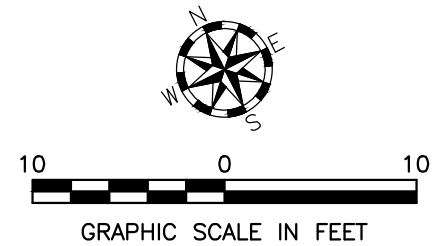
REV.	DESCRIPTION	APPR.	DATE



Drawn By	: V.M.
Checked By	: R.T.P.
Approved By	: R.T.P.
Project No.	: 5740102
Scale	: AS NOTED
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Revision	: 0



**ADA BUS STOP IMPROVEMENTS
PHASE VI**
BUS STOP 908 - MCARDLE AT JOANN
REGIONAL TRANSPORTATION AUTHORITY
CORPUS CHRISTI, TEXAS



NEW CONCRETE SHELTER PAD	240	SF
NEW SIDEWALK SECTION	50	SF
SAW CUT CONCRETE PAVEMENT (FULL DEPTH)	8	LF
DEMOLITION OF EXISTING SIDEWALK	160	SF
6" HEADER CURB	40	LF
BUS STOP CURB YELLOW STRIPING	60	LF

NOTES:

M.E. = MATCH EXISTING ELEVATION

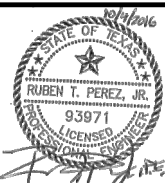
CROSS SLOPES FOR ALL CURB RAMPS, LANDINGS, SIDEWALKS, AND BUS STOP SHELTER PADS SHALL BE A MAXIMUM OF 2.0%

KEY NOTES:

- ① CONCRETE CURB RAMPS = 8.3% MAX SLOPE
- ② CONCRETE SIDEWALKS = 5.0% MAX SLOPE
- ③ CONCRETE LANDINGS = 2.0% MAX SLOPE ALL DIRECTIONS
- ④ CONCRETE BUS STOP SHELTER PAD = 2.0% MAX SLOPE ALL DIRECTIONS

BUS HEADING SOUTHEAST
BUS STOP SIGN ID 909

REV.	DESCRIPTION	APPR.	DATE



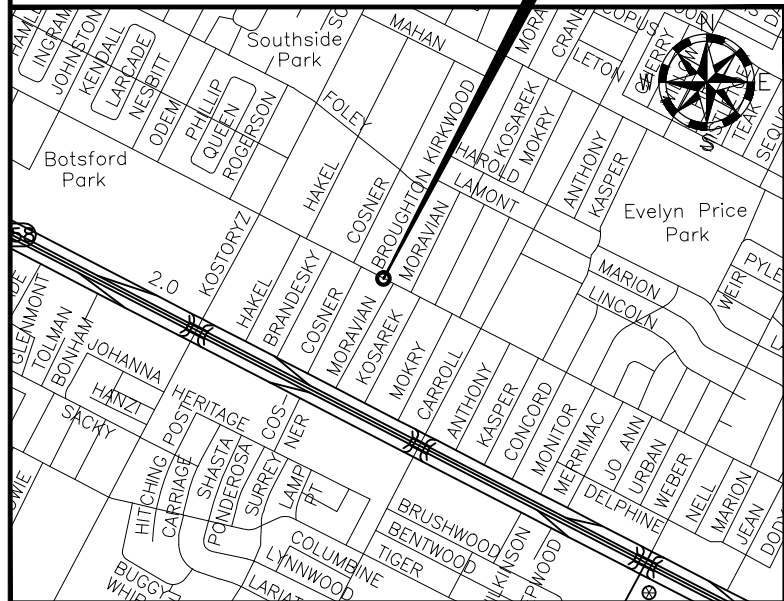
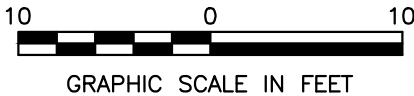
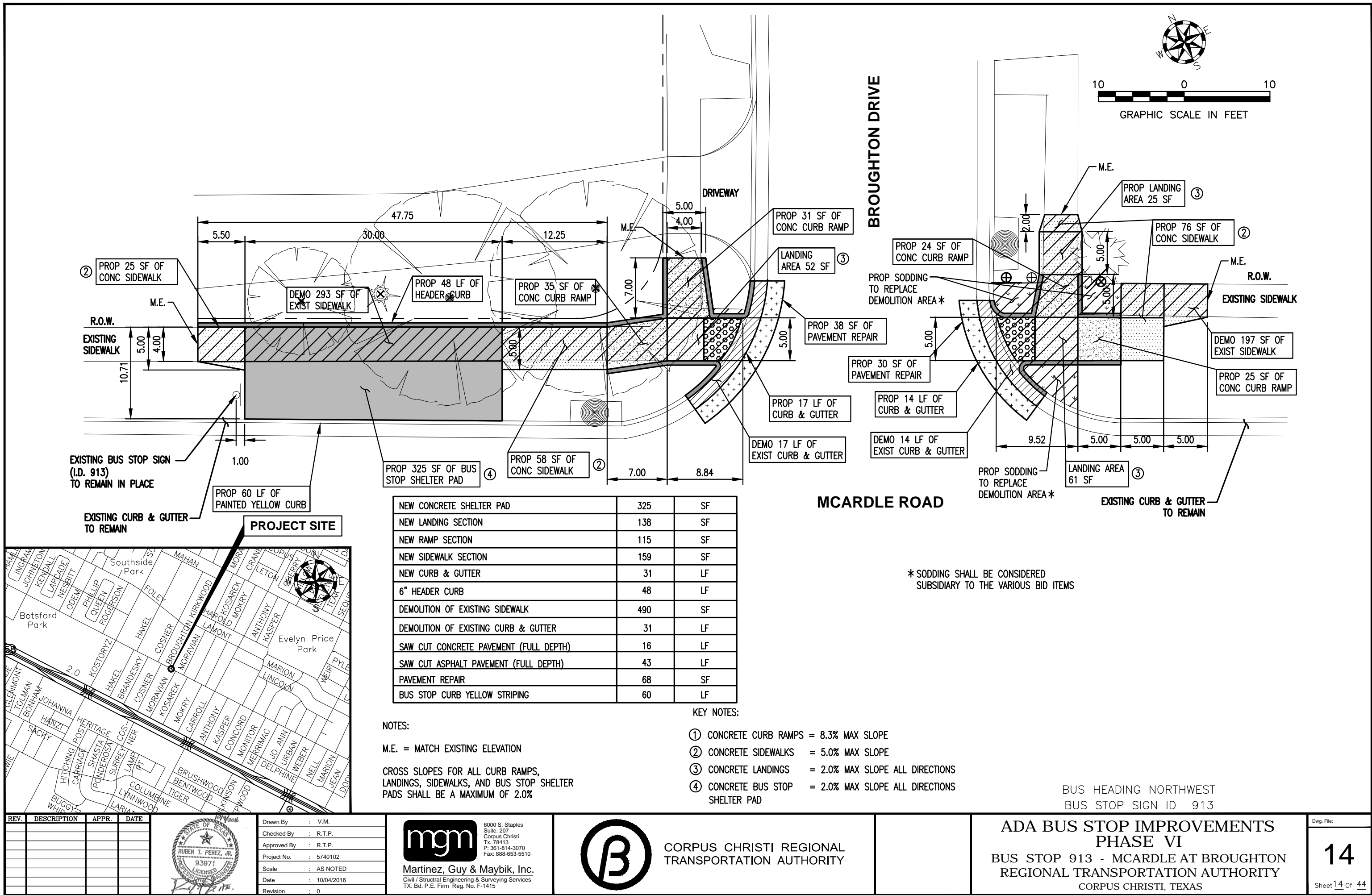
Drawn By	: V.M.
Checked By	: R.T.P.
Approved By	: R.T.P.
Project No.	: 5740102
Scale	: AS NOTED
Date	: 10/04/2016
Revision	: 0

mgm
6000 S. Staples
Suite 207
Corpus Christi
Tx 78413
P: 361-814-3070
Fax: 888-653-5510

Martinez, Guy & Maybik, Inc.
Civil / Structural Engineering & Surveying Services
TX. Bd. P.E. Firm Reg. No. F-1415

B
CORPUS CHRISTI REGIONAL
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**ADA BUS STOP IMPROVEMENTS
PHASE VI**
BUS STOP 909 - MCARDLE AT KARCHMER
REGIONAL TRANSPORTATION AUTHORITY
CORPUS CHRISTI, TEXAS



NEW CONCRETE SHELTER PAD	325	SF
NEW LANDING SECTION	138	SF
NEW RAMP SECTION	115	SF
NEW SIDEWALK SECTION	159	SF
NEW CURB & GUTTER	31	LF
6" HEADER CURB	48	LF
DEMOLITION OF EXISTING SIDEWALK	490	SF
DEMOLITION OF EXISTING CURB & GUTTER	31	LF
SAW CUT CONCRETE PAVEMENT (FULL DEPTH)	16	LF
SAW CUT ASPHALT PAVEMENT (FULL DEPTH)	43	LF
PAVEMENT REPAIR	68	SF
BUS STOP CURB YELLOW STRIPING	60	LF

NOTES:

M.E. = MATCH EXISTING ELEVATION

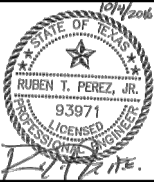
CROSS SLOPES FOR ALL CURB RAMPS, LANDINGS, SIDEWALKS, AND BUS STOP SHELTER PADS SHALL BE A MAXIMUM OF 2.0%

- KEY NOTES:
- ① CONCRETE CURB RAMPS = 8.3% MAX SLOPE
 - ② CONCRETE SIDEWALKS = 5.0% MAX SLOPE
 - ③ CONCRETE LANDINGS = 2.0% MAX SLOPE ALL DIRECTIONS
 - ④ CONCRETE BUS STOP = 2.0% MAX SLOPE ALL DIRECTIONS SHELTER PAD

* SODDING SHALL BE CONSIDERED
SUBSIDIARY TO THE VARIOUS BID ITEMS

BUS HEADING NORTHWEST
BUS STOP SIGN ID 913

REV.	DESCRIPTION	APPR.	DATE



Drawn By	: V.M.
Checked By	: R.T.P.
Approved By	: R.T.P.
Project No.	: 5740102
Scale	: AS NOTED
Date	: 10/04/2016
Revision	: 0



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6000 S. Staples
Suite 207
Corpus Christi
TX 78413
P: 361-814-3070
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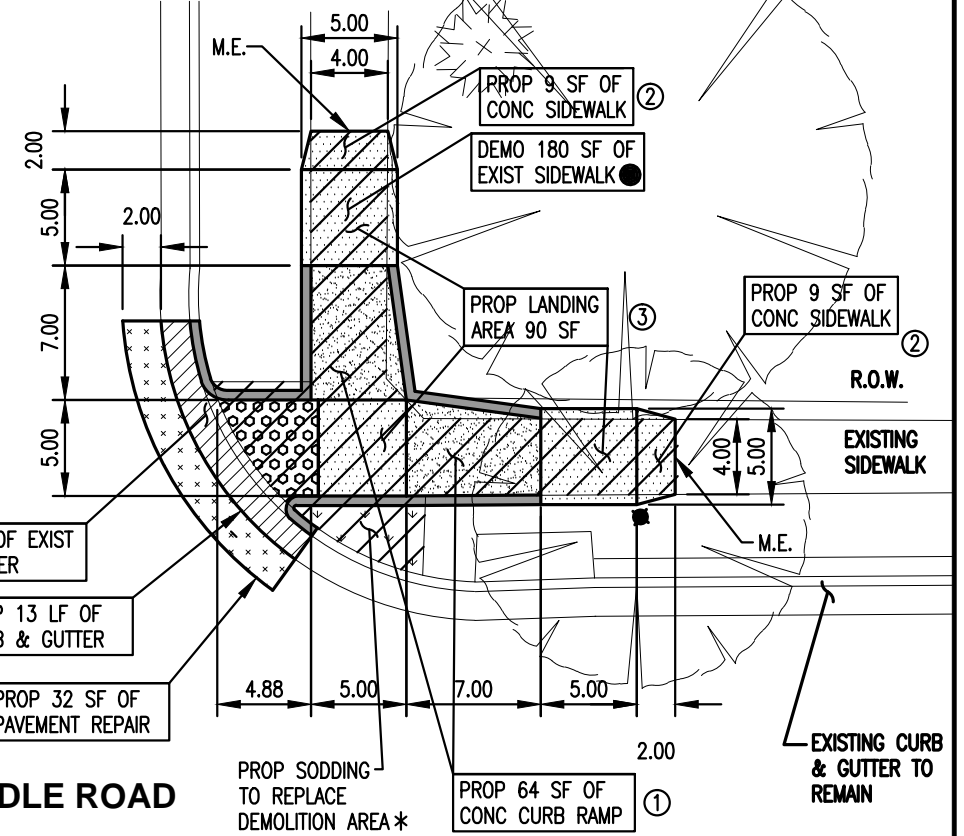
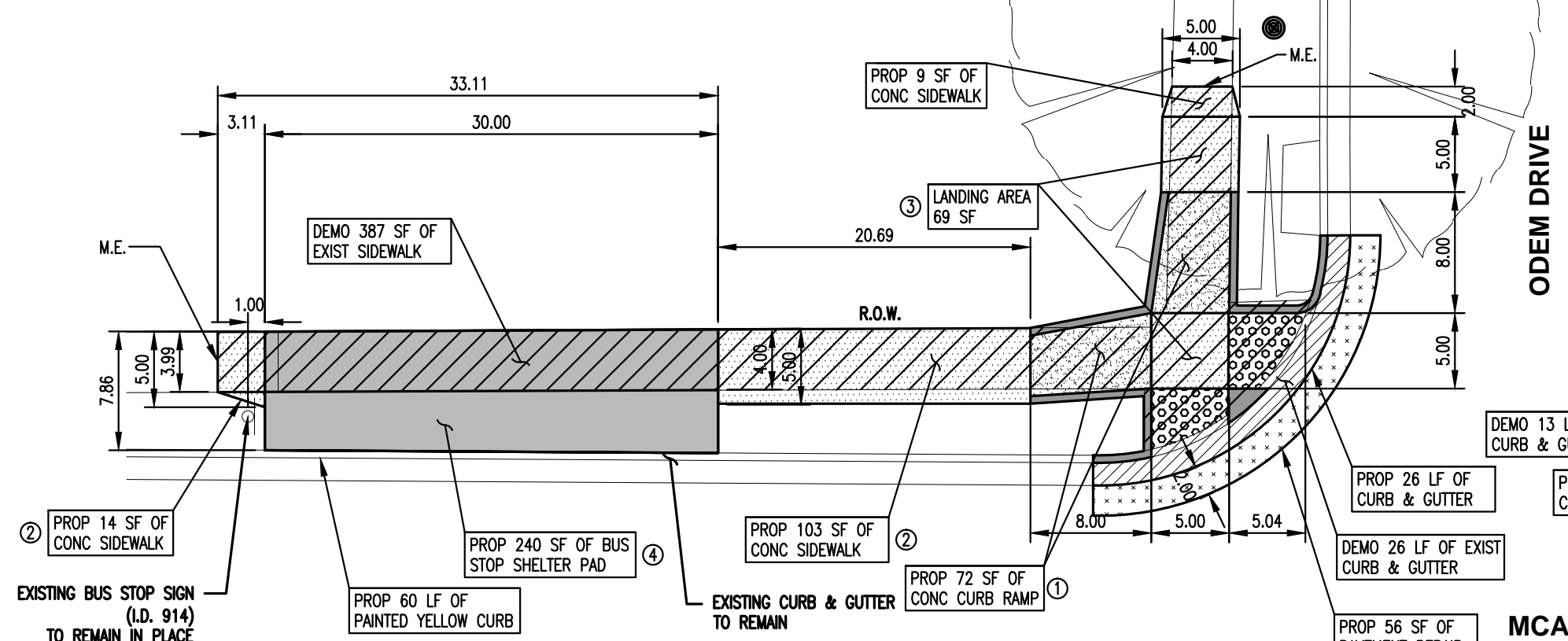
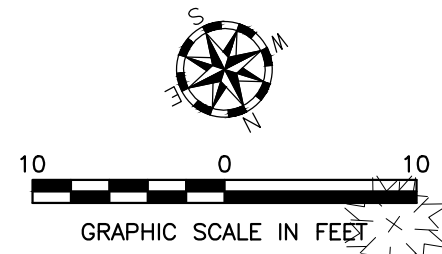
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TRANSPORTATION AUTHORITY

ADA BUS STOP IMPROVEMENTS
PHASE VI
BUS STOP 913 - MCARDLE AT BROUGHTON
REGIONAL TRANSPORTATION AUTHORITY
CORPUS CHRISTI, TEXAS

Dwg. File:

14

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MCARDLE ROAD

* SODDING SHALL BE CONSIDERED
SUBSIDIARY TO THE VARIOUS BID ITEMS

NEW CONCRETE SHELTER PAD	240	SF
NEW LANDING SECTION	159	SF
NEW SIDEWALK SECTION	144	SF
NEW RAMP SECTION	136	SF
NEW CURB & GUTTER	39	LF
DEMOLITION OF EXISTING SIDEWALK	567	SF
DEMOLITION OF EXISTING CURB & GUTTER	39	LF
SAW CUT CONCRETE PAVEMENT (FULL DEPTH)	16	LF
SAW CUT ASPHALT PAVEMENT (FULL DEPTH)	51	LF
PAVEMENT REPAIR	88	SF
BUS STOP CURB YELLOW STRIPING	60	LF

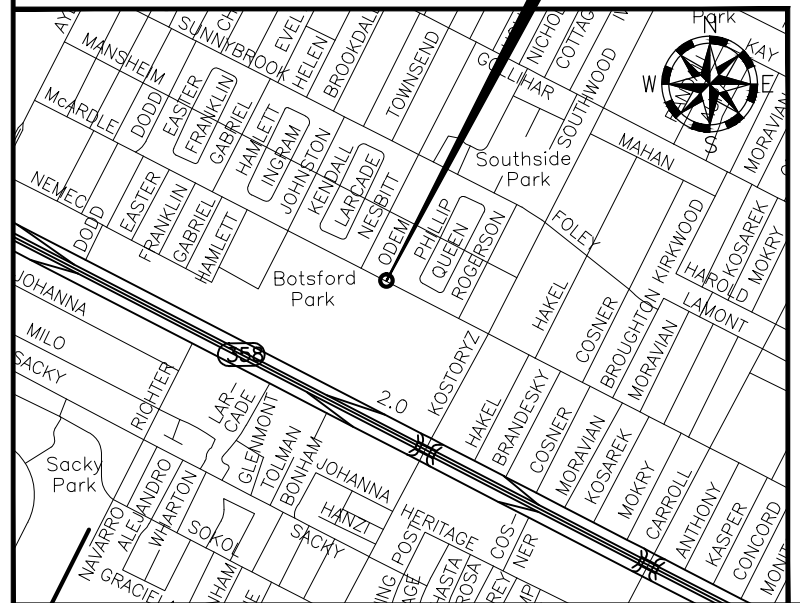
NOTES:

M.E. = MATCH EXISTING ELEVATION

CROSS SLOPES FOR ALL CURB RAMPS,
LANDINGS, SIDEWALKS, AND BUS STOP SHELTER
PADS SHALL BE A MAXIMUM OF 2.0%

KEY NOTES:

- ① CONCRETE CURB RAMPS = 8.3% MAX SLOPE
 - ② CONCRETE SIDEWALKS = 5.0% MAX SLOPE
 - ③ CONCRETE LANDINGS = 2.0% MAX SLOPE ALL DIRECTIONS
 - ④ CONCRETE BUS STOP = 2.0% MAX SLOPE ALL DIRECTIONS
- SHELTER PAD



BUS HEADING NORTHWEST
BUS STOP SIGN ID 916

REV.	DESCRIPTION	APPR.	DATE

Drawn By	: V.M.
Checked By	: R.T.P.
Approved By	: R.T.P.
Project No.	: 5740102
Scale	: AS NOTED
Date	: 10/04/2016
Revision	: 0

mgm
6000 S. Staples
Suite 207
Corpus Christi
Tx. 78413
P: 361-814-3070
Fax: 888-653-5510

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Civil / Structural Engineering & Surveying Services
TX. Bd. P.E. Firm Reg. No. F-1415

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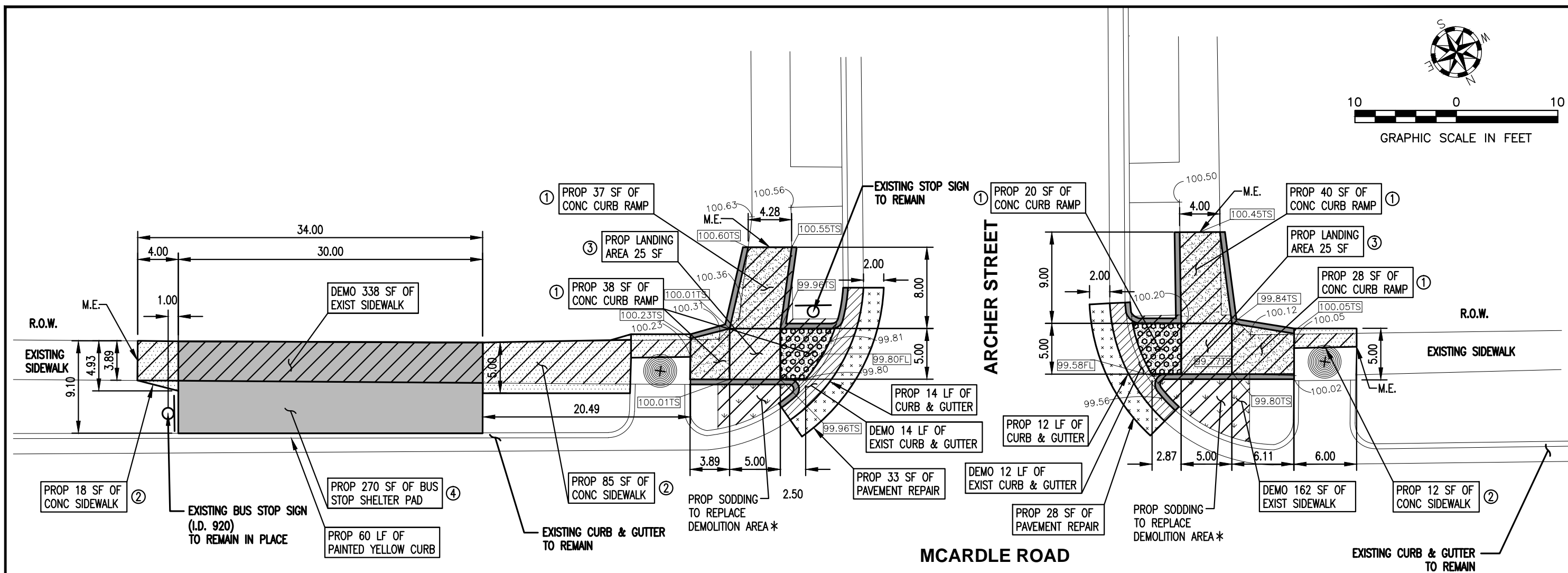


**ADA BUS STOP IMPROVEMENTS
PHASE VI**
BUS STOP 916 - MCARDLE AT ODEM
REGIONAL TRANSPORTATION AUTHORITY
CORPUS CHRISTI, TEXAS

Dwg. File:

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NEW CONCRETE SHELTER PAD	270	SF
NEW LANDING SECTION	50	SF
NEW RAMP SECTION	163	SF
NEW SIDEWALK SECTION	115	SF
NEW CURB & GUTTER	26	LF
DEMOLITION OF EXISTING SIDEWALK	500	SF
DEMOLITION OF EXISTING CURB & GUTTER	26	LF
SAW CUT CONCRETE PAVEMENT (FULL DEPTH)	14	LF
SAW CUT ASPHALT PAVEMENT (FULL DEPTH)	38	LF
PAVEMENT REPAIR	61	SF
BUS STOP CURB YELLOW STRIPING	60	LF

PROJECT SITE

Map showing the project site location in St. Louis, Missouri. The site is located on the east side of Highway 288, between the intersection with Highway 380 and the intersection with Highway 288. The map includes street names such as Castenon, Muddy, Mistry, Prescott, Mansheim, Garden, Inwood Park, Trojan, Fountain Park, Hill, Johanna, Milo, Sacky, Hygeia, Gibbs, Richter, Lap-Cade, Mont, Koston, Phillip, Queen, ODEM, Nesselbitt, Lardade, Kendall, Johnston, Ingram, Hamlett, Gabriel, Franklin, Easter, Dodd, McRae, Ayers, Mansheim, Cheryll, Blundell, Archer, Kilo, Carlo, Dinn, Viola, Sunnybrook, Evelyn, Helen, Brookdale, Townsend, Green, Gray, and Botsford Park. A compass rose is located in the upper right corner, and a scale bar is in the lower right corner.

BUS HEADING NORTHWEST
BUS STOP SIGN ID 920

[illegible]

Drawn By	:	V.M.
Checked By	:	R.T.P.
Approved By	:	R.T.P.
Project No.	:	5740102
Scale	:	AS NOTED
Date	:	10/04/2016
Revision	:	0

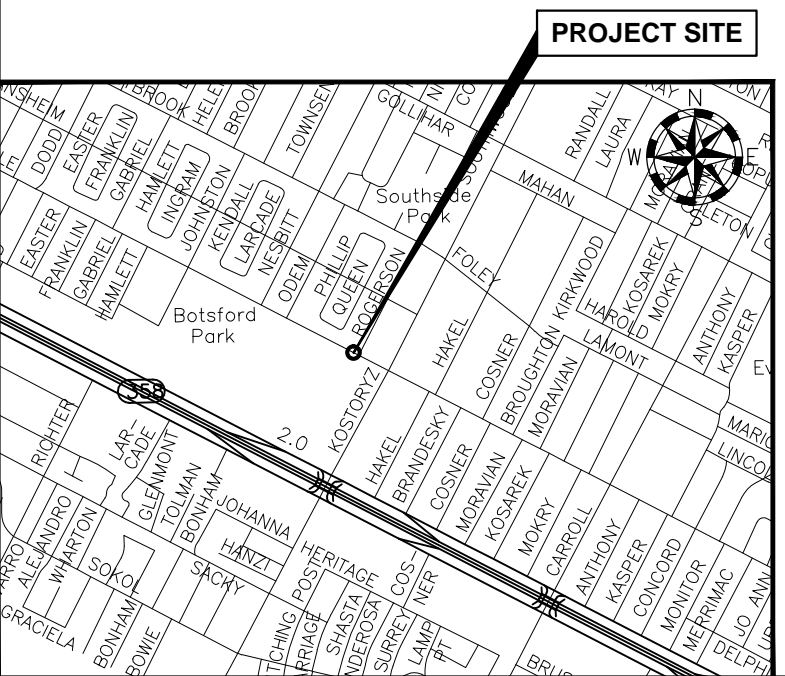
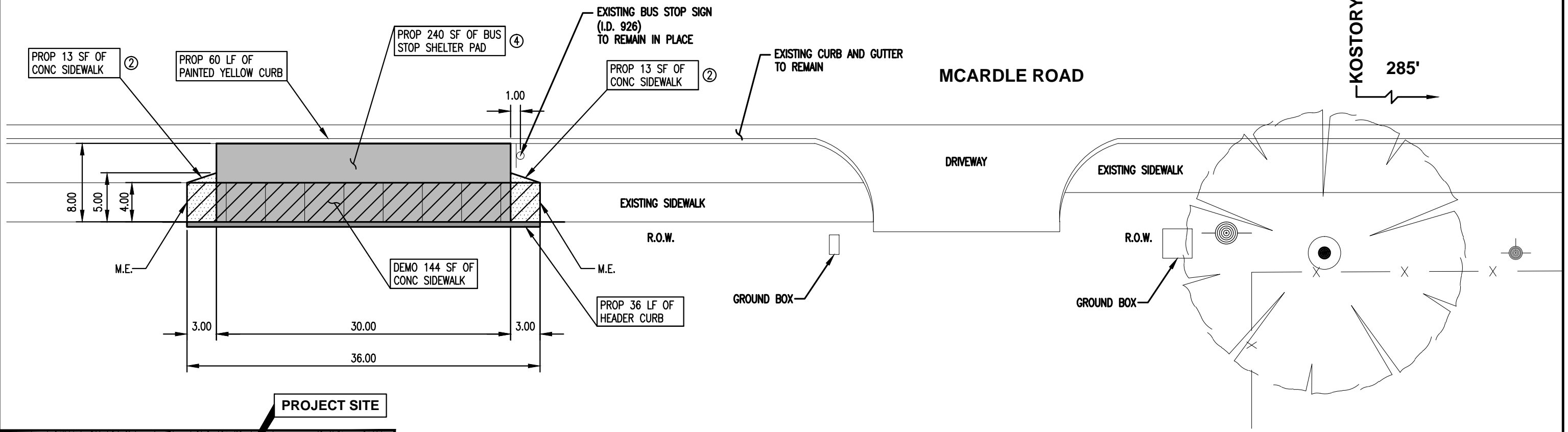
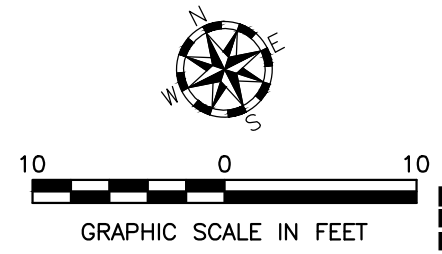


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**ADA BUS STOP IMPROVEMENTS
PHASE VI
BUS STOP 920 - MCARDLE AT ARCHER
REGIONAL TRANSPORTATION AUTHORITY
CORPUS CHRISTI, TEXAS**



NEW CONCRETE SHELTER PAD	240	SF
NEW SIDEWALK SECTION	26	SF
SAW CUT CONCRETE PAVEMENT (FULL DEPTH)	8	LF
DEMOLITION OF EXISTING SIDEWALK	144	SF
6" HEADER CURB	36	LF
BUS STOP CURB YELLOW STRIPING	60	LF

NOTES:

M.E. = MATCH EXISTING ELEVATION

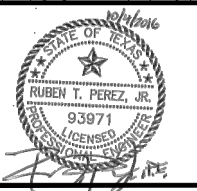
CROSS SLOPES FOR ALL CURB RAMPS, LANDINGS, SIDEWALKS, AND BUS STOP SHELTER PADS SHALL BE A MAXIMUM OF 2.0%

KEY NOTES:

- ① CONCRETE CURB RAMPS = 8.3% MAX SLOPE
- ② CONCRETE SIDEWALKS = 5.0% MAX SLOPE
- ③ CONCRETE LANDINGS = 2.0% MAX SLOPE ALL DIRECTIONS
- ④ CONCRETE BUS STOP = 2.0% MAX SLOPE ALL DIRECTIONS SHELTER PAD

BUS HEADING SOUTHEAST
BUS STOP SIGN ID 926

REV.	DESCRIPTION	APPR.	DATE




Drawn By	: V.M.
Checked By	: R.T.P.
Approved By	: R.T.P.
Project No.	: 5740102
Scale	: AS NOTED
Date	: 10/04/2016
Revision	: 0



6000 S. Staples
Suite 207
Corpus Christi
Tx. 78413
P: 361-814-3070
Fax: 888-653-5510

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Civil / Structural Engineering & Surveying Services
TX. Bd. P.E. Firm Reg. No. F-1415



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**ADA BUS STOP IMPROVEMENTS
PHASE VI**

BUS STOP 926 - MCARDLE AT ROGERSON

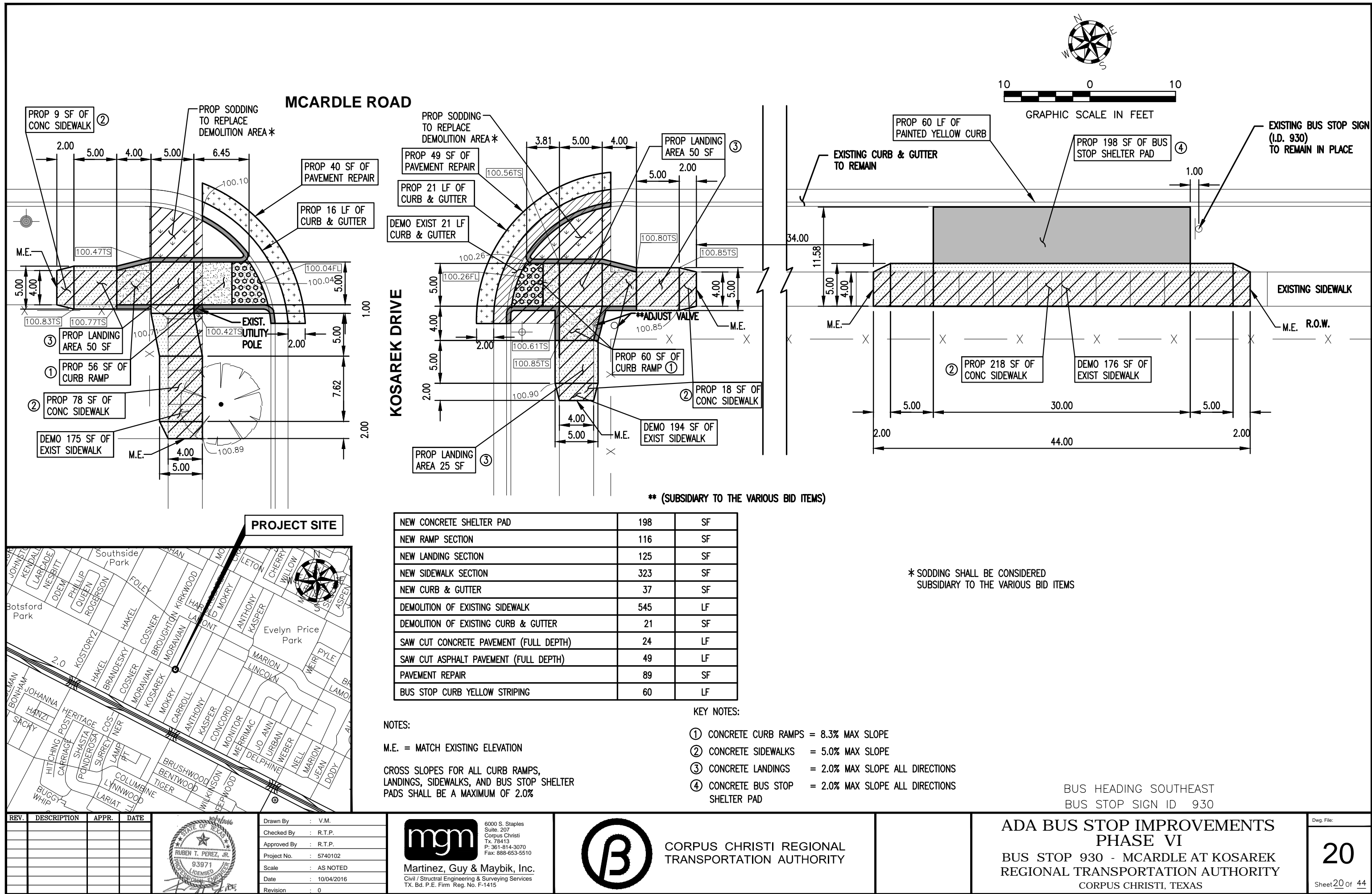
REGIONAL TRANSPORTATION AUTHORITY

CORPUS CHRISTI, TEXAS

Dwg. File:

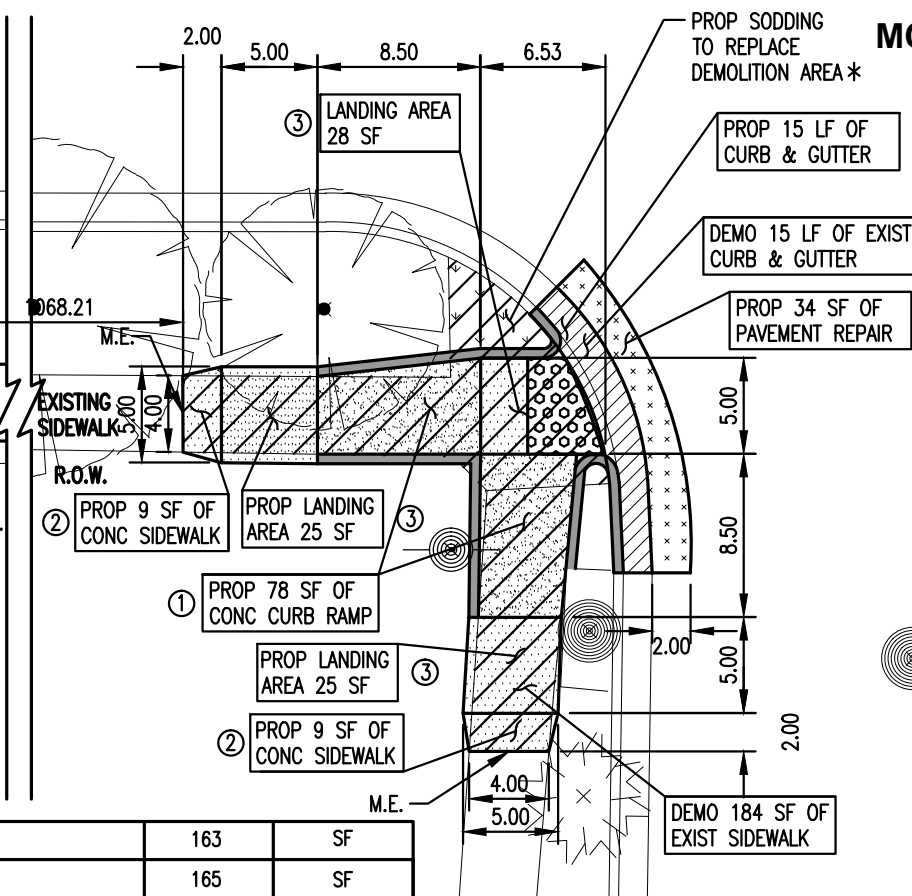
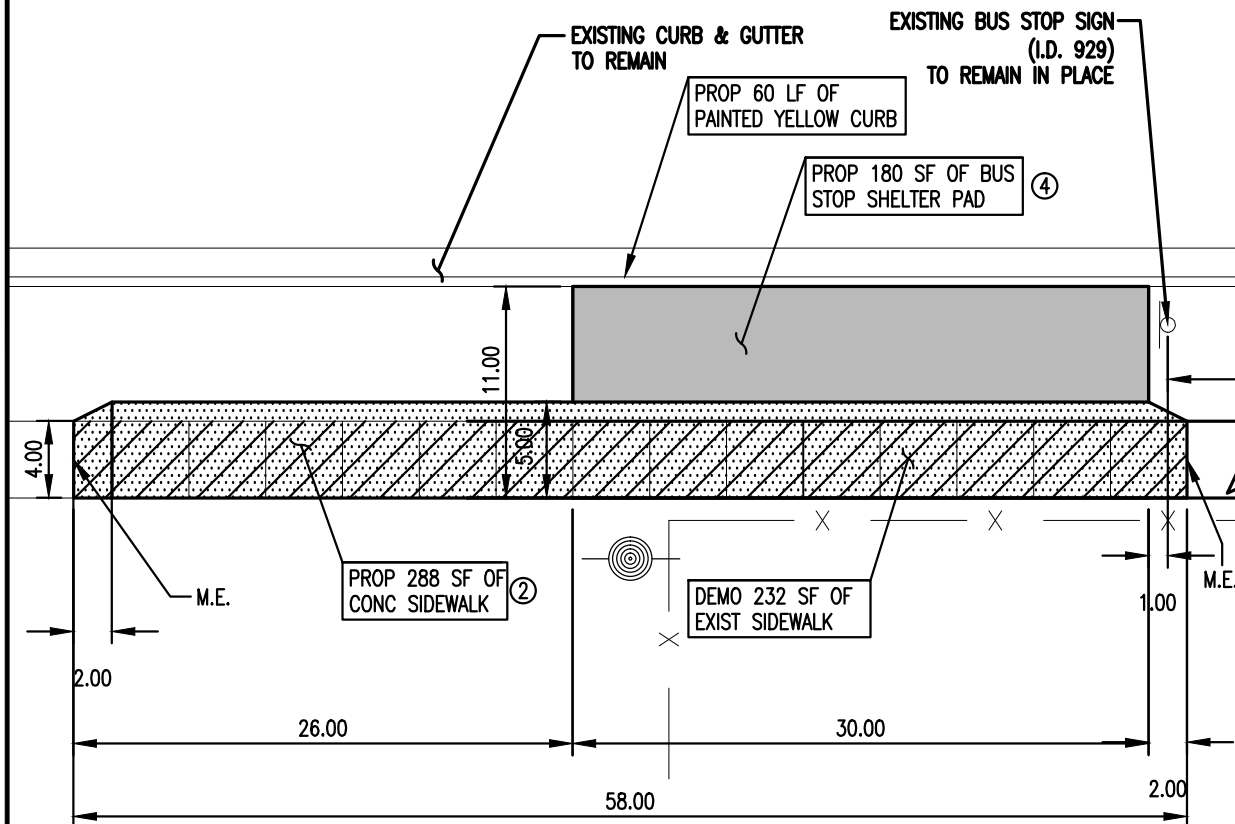
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Sheet 19 of 44



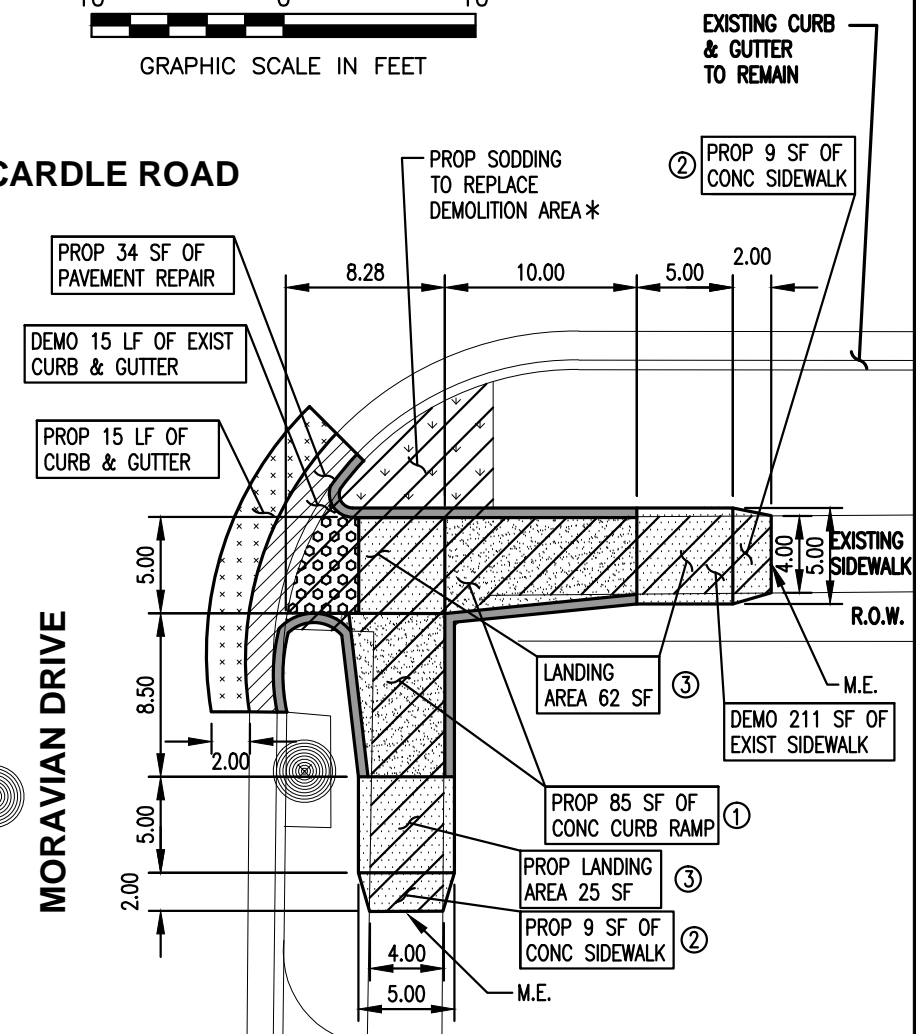


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GRAPHIC SCALE IN FEET

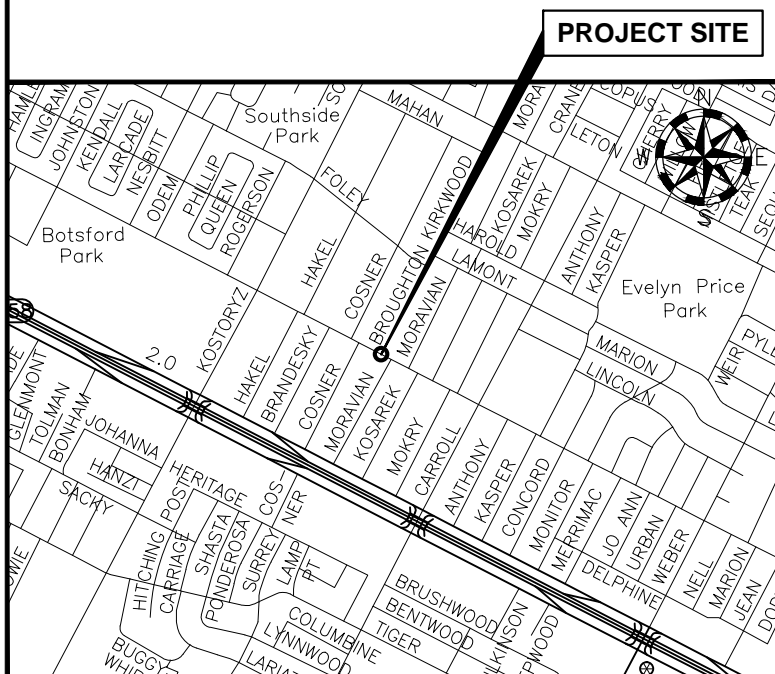


MCARDLE ROAD

MORAVIAN DRIVE



* SODDING SHALL BE CONSIDERED
SUBSIDIARY TO THE VARIOUS BID ITEMS



NEW RAMP SECTION	163	SF
NEW LANDING SECTION	165	SF
NEW CONCRETE SHELTER PAD	180	SF
NEW SIDEWALK SECTION	324	SF
NEW CURB & GUTTER	30	SF
DEMOLITION OF EXISTING SIDEWALK	627	SF
DEMOLITION OF EXISTING CURB & GUTTER	30	LF
SAW CUT CONCRETE PAVEMENT (FULL DEPTH)	24	LF
SAW CUT ASPHALT PAVEMENT (FULL DEPTH)	42	LF
PAVEMENT REPAIR	68	LF
BUS STOP CURB YELLOW STRIPING	60	LF

NOTES:

M.E. = MATCH EXISTING ELEVATION

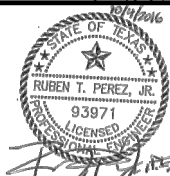
CROSS SLOPES FOR ALL CURB RAMPS,
LANDINGS, SIDEWALKS, AND BUS STOP SHELTER
PADS SHALL BE A MAXIMUM OF 2.0%

KEY NOTES:

- ① CONCRETE CURB RAMPS = 8.3% MAX SLOPE
- ② CONCRETE SIDEWALKS = 5.0% MAX SLOPE
- ③ CONCRETE LANDINGS = 2.0% MAX SLOPE ALL DIRECTIONS
- ④ CONCRETE BUS STOP = 2.0% MAX SLOPE ALL DIRECTIONS SHELTER PAD

BUS HEADING SOUTHEAST
BUS STOP SIGN ID 929

REV.	DESCRIPTION	APPR.	DATE



Drawn By	: V.M.
Checked By	: R.T.P.
Approved By	: R.T.P.
Project No.	: 5740102
Scale	: AS NOTED
Date	: 10/04/2016
Revision	: 0



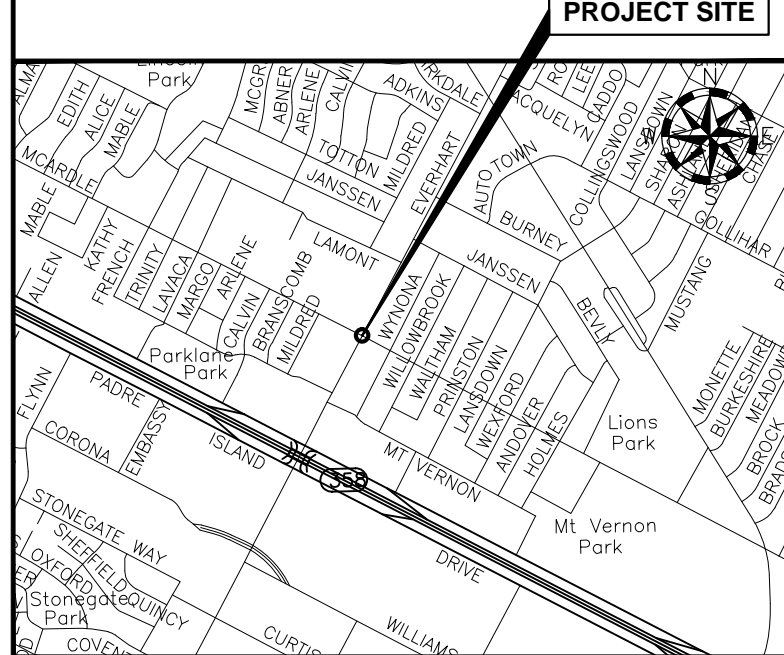
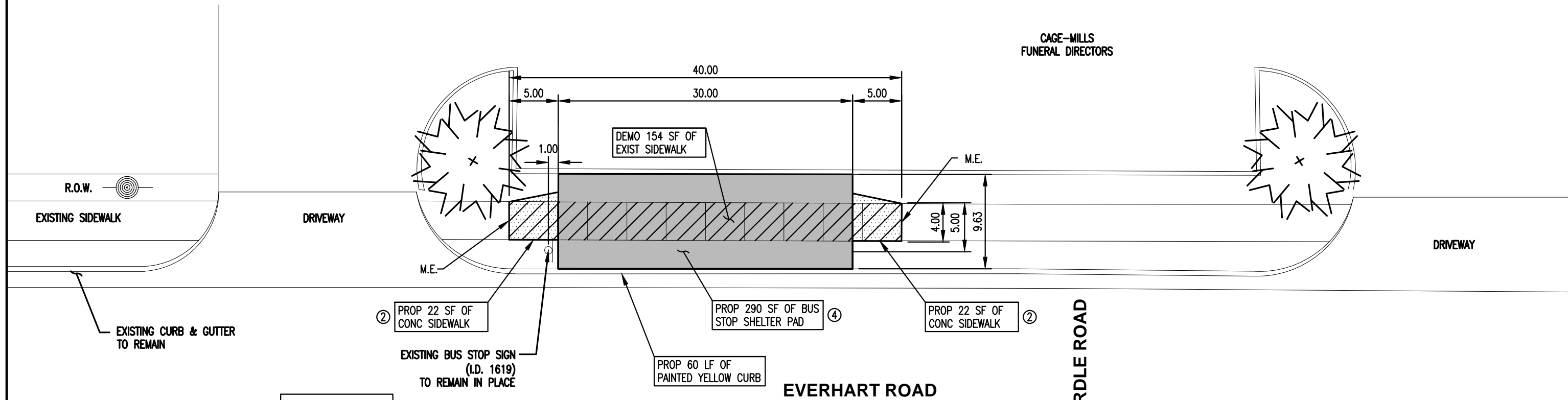
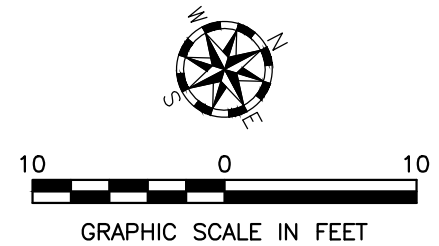
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TRANSPORTATION AUTHORITY

ADA BUS STOP IMPROVEMENTS
PHASE VI
BUS STOP 929 - MCARDLE AT BROUGHTON
REGIONAL TRANSPORTATION AUTHORITY
CORPUS CHRISTI, TEXAS

Dwg. File:
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NEW CONCRETE SHELTER PAD	290	SF
NEW SIDEWALK SECTION	44	SF
DEMOLITION OF EXISTING SIDEWALK	154	SF
SAW CUT CONCRETE PAVEMENT (FULL DEPTH)	8	LF
BUS STOP CURB YELLOW STRIPING	60	LF

NOTES:

M.E. = MATCH EXISTING ELEVATION

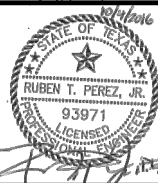
CROSS SLOPES FOR ALL CURB RAMPS, LANDINGS, SIDEWALKS, AND BUS STOP SHELTER PADS SHALL BE A MAXIMUM OF 2.0%

KEY NOTES:

- ① CONCRETE CURB RAMPS = 8.3% MAX SLOPE
- ② CONCRETE SIDEWALKS = 5.0% MAX SLOPE
- ③ CONCRETE LANDINGS = 2.0% MAX SLOPE ALL DIRECTIONS
- ④ CONCRETE BUS STOP = 2.0% MAX SLOPE ALL DIRECTIONS SHELTER PAD

BUS HEADING SOUTHWEST
BUS STOP SIGN ID 1619

REV.	DESCRIPTION	APPR.	DATE



Drawn By	: V.M.
Checked By	: R.T.P.
Approved By	: R.T.P.
Project No.	: 5740102
Scale	: AS NOTED
Date	: 10/04/2016
Revision	: 0



6000 S. Staples
Suite 207
Corpus Christi
TX 78413
P: 361-814-3070
Fax: 888-653-5510
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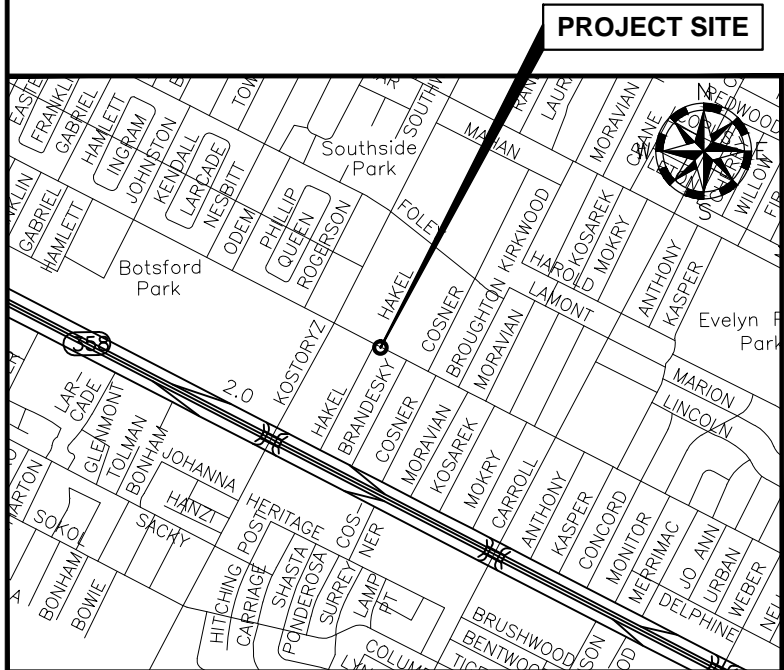
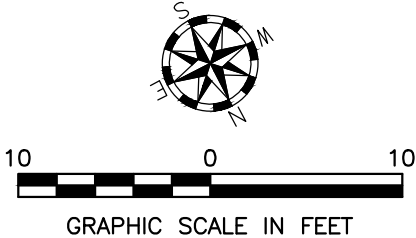
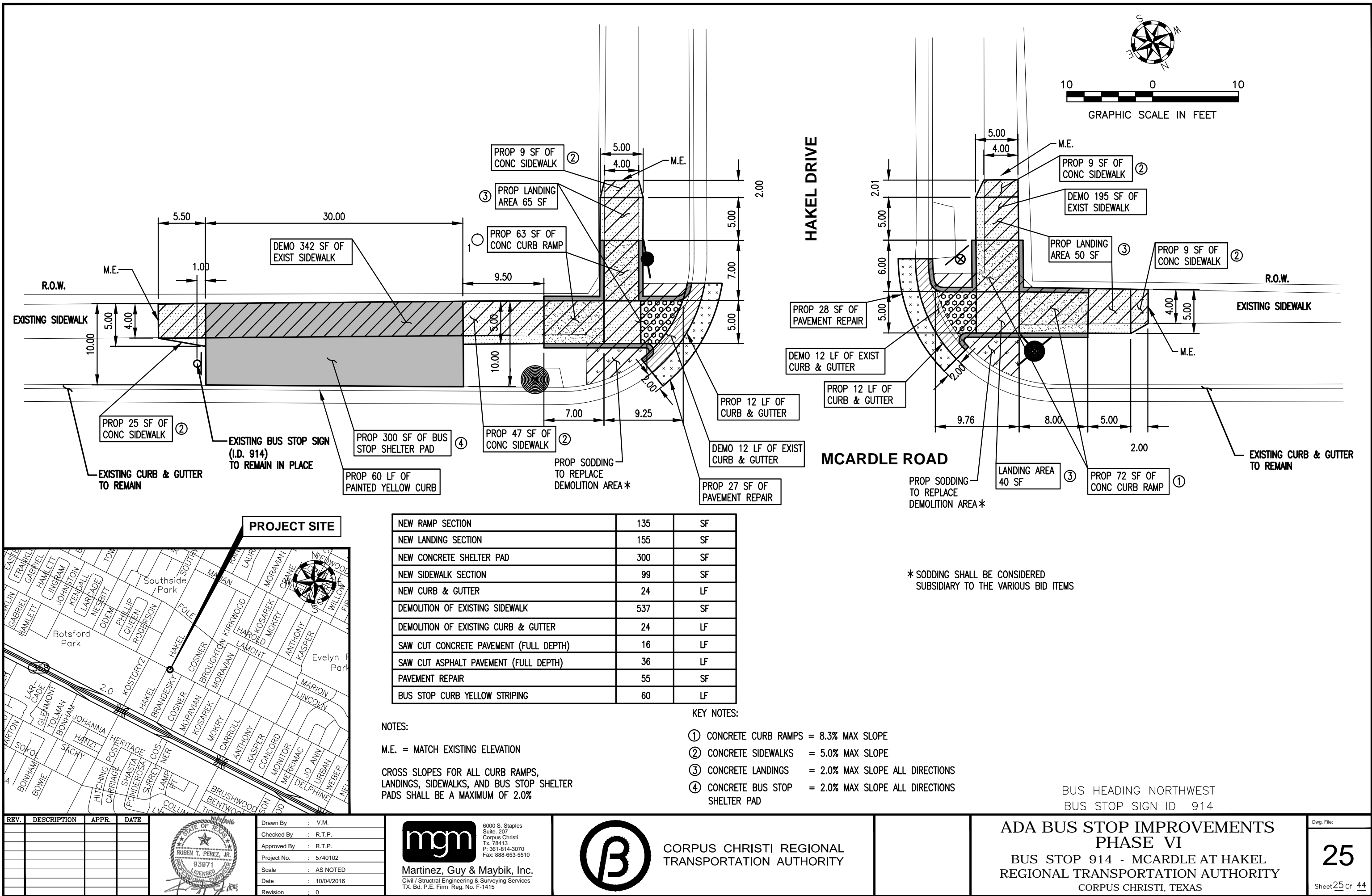
CORPUS CHRISTI REGIONAL
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**ADA BUS STOP IMPROVEMENTS
PHASE VI**
BUS STOP 1619 - EVERHART AT MCARDLE
REGIONAL TRANSPORTATION AUTHORITY
CORPUS CHRISTI, TEXAS

Dwg. File:

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NEW RAMP SECTION	135	SF
NEW LANDING SECTION	155	SF
NEW CONCRETE SHELTER PAD	300	SF
NEW SIDEWALK SECTION	99	SF
NEW CURB & GUTTER	24	LF
DEMOLITION OF EXISTING SIDEWALK	537	SF
DEMOLITION OF EXISTING CURB & GUTTER	24	LF
SAW CUT CONCRETE PAVEMENT (FULL DEPTH)	16	LF
SAW CUT ASPHALT PAVEMENT (FULL DEPTH)	36	LF
PAVEMENT REPAIR	55	SF
BUS STOP CURB YELLOW STRIPING	60	LF

NOTES:

M.E. = MATCH EXISTING ELEVATION

CROSS SLOPES FOR ALL CURB RAMPS, LANDINGS, SIDEWALKS, AND BUS STOP SHELTER PADS SHALL BE A MAXIMUM OF 2.0%

KEY NOTES:

- ① CONCRETE CURB RAMPS = 8.3% MAX SLOPE
- ② CONCRETE SIDEWALKS = 5.0% MAX SLOPE
- ③ CONCRETE LANDINGS = 2.0% MAX SLOPE ALL DIRECTIONS
- ④ CONCRETE BUS STOP = 2.0% MAX SLOPE ALL DIRECTIONS

* SODDING SHALL BE CONSIDERED
SUBSIDIARY TO THE VARIOUS BID ITEMS

BUS HEADING NORTHWEST
BUS STOP SIGN ID 914

REV.	DESCRIPTION	APPR.	DATE

Drawn By : V.M.
Checked By : R.T.P.
Approved By : R.T.P.
Project No. : 5740102
Scale : AS NOTED
Date : 10/04/2016
Revision : 0

6000 S. Staples
Suite 207
Corpus Christi
TX 78413
P: 361-814-3070
Fax: 888-653-5510

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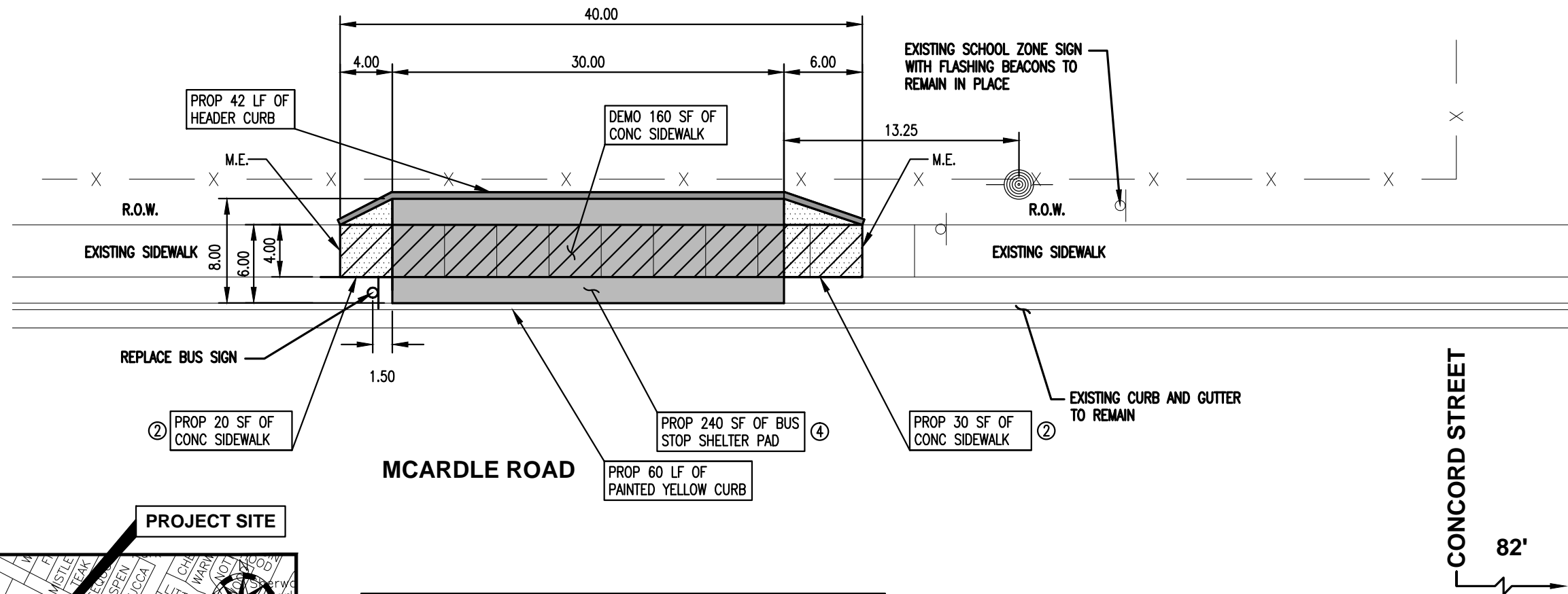
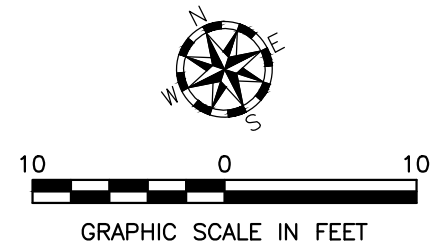
**ADA BUS STOP IMPROVEMENTS
PHASE VI**

BUS STOP 914 - MCARDLE AT HAKEL
REGIONAL TRANSPORTATION AUTHORITY
CORPUS CHRISTI, TEXAS

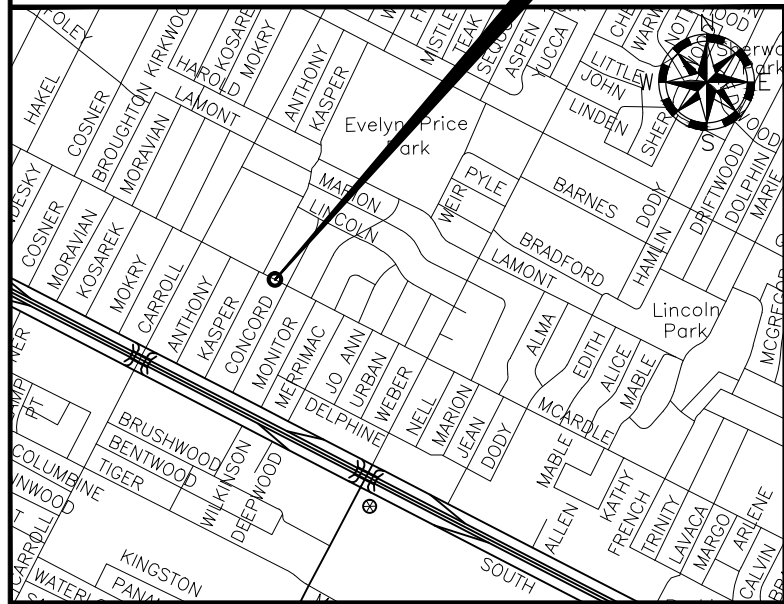
Dwg. File:

25

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PROJECT SITE



NEW CONCRETE SHELTER PAD	240	SF
NEW SIDEWALK SECTION	50	SF
DEMOLITION OF EXISTING SIDEWALK	160	SF
SAW CUT CONCRETE PAVEMENT (FULL DEPTH)	8	LF
6" HEADER CURB	42	LF
RELOCATE EXISTING BUS STOP SIGN	1	EA
BUS STOP CURB YELLOW STRIPING	60	LF

NOTES:

M.E. = MATCH EXISTING ELEVATION

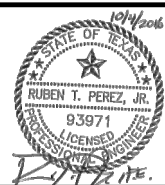
CROSS SLOPES FOR ALL CURB RAMPS, LANDINGS, SIDEWALKS, AND BUS STOP SHELTER PADS SHALL BE A MAXIMUM OF 2.0%

KEY NOTES:

- ① CONCRETE CURB RAMPS = 8.3% MAX SLOPE
- ② CONCRETE SIDEWALKS = 5.0% MAX SLOPE
- ③ CONCRETE LANDINGS = 2.0% MAX SLOPE ALL DIRECTIONS
- ④ CONCRETE BUS STOP = 2.0% MAX SLOPE ALL DIRECTIONS SHELTER PAD

BUS HEADING SOUTHEAST
BUS STOP SIGN ID 910

REV.	DESCRIPTION	APPR.	DATE



Drawn By	: V.M.
Checked By	: R.T.P.
Approved By	: R.T.P.
Project No.	: 5740102
Scale	: AS NOTED
Date	: 10/04/2016
Revision	: 0

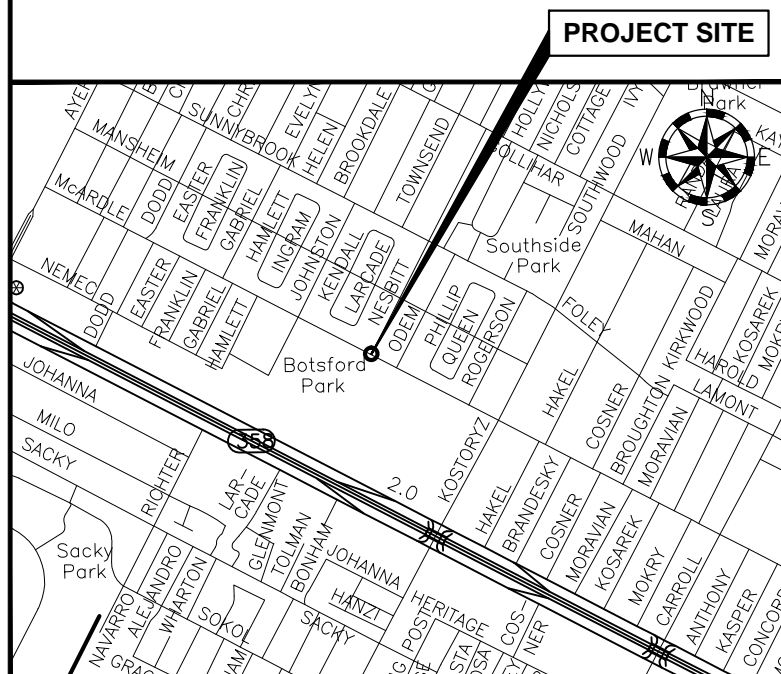
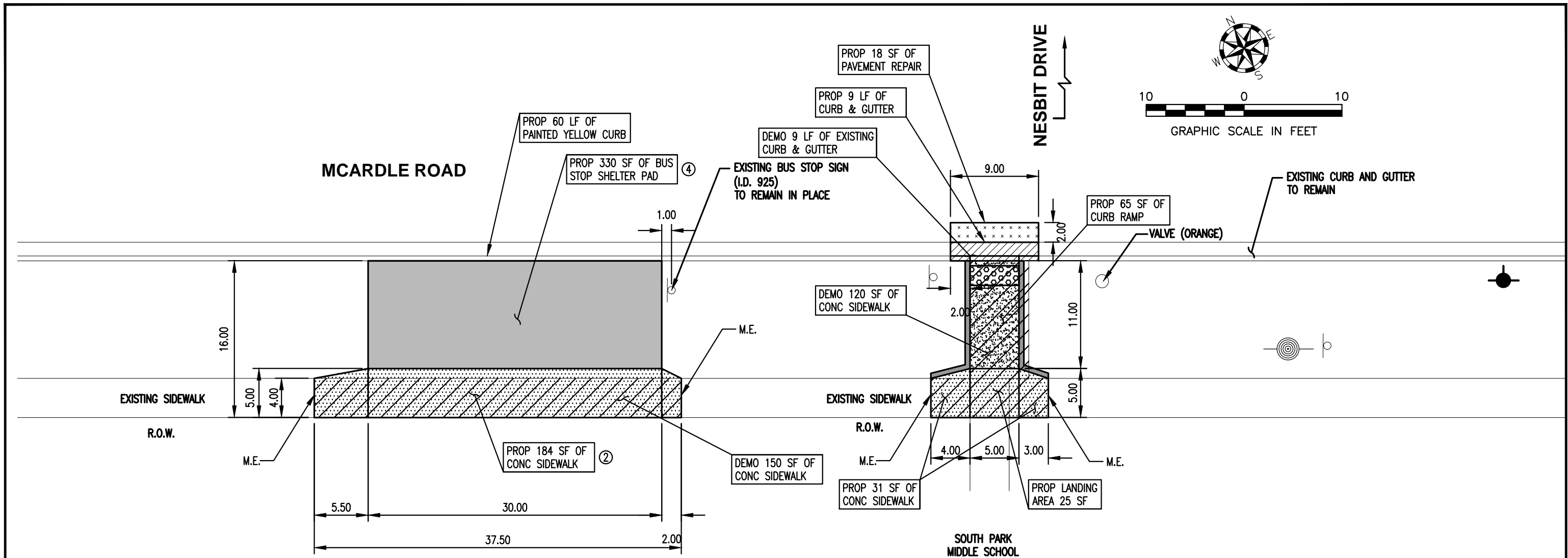
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6000 S. Staples
Suite 207
Corpus Christi
Tx 78413
P: 361-814-3070
Fax: 888-653-5510
Martinez, Guy & Maybik, Inc.
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TX. Bd. P.E. Firm Reg. No. F-1415

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**ADA BUS STOP IMPROVEMENTS
PHASE VI**
BUS STOP 910 - MCARDLE AT CONCORD
REGIONAL TRANSPORTATION AUTHORITY
CORPUS CHRISTI, TEXAS

Dwg. File:
26
Sheet 26 of 44



NEW RAMP SECTION	65	SF
NEW LANDING SECTION	25	SF
NEW CONCRETE SHELTER PAD	330	SF
NEW SIDEWALK SECTION	215	SF
NEW CURB & GUTTER	9	LF
DEMOLITION OF EXISTING SIDEWALK	270	LF
DEMOLITION OF EXISTING CURB & GUTTER	9	LF
SAW CUT CONCRETE PAVEMENT (FULL DEPTH)	16	LF
SAW CUT ASPHALT PAVEMENT (FULL DEPTH)	13	LF
PAVEMENT REPAIR	18	SF
BUS STOP CURB YELLOW STRIPING	60	LF

NOTES:

M.E. = MATCH EXISTING ELEVATION

CROSS SLOPES FOR ALL CURB RAMPS, LANDINGS, SIDEWALKS, AND BUS STOP SHELTER PADS SHALL BE A MAXIMUM OF 2.0%

KEY NOTES:

- ① CONCRETE CURB RAMPS = 8.3% MAX SLOPE
- ② CONCRETE SIDEWALKS = 5.0% MAX SLOPE
- ③ CONCRETE LANDINGS = 2.0% MAX SLOPE ALL DIRECTIONS
- ④ CONCRETE BUS STOP SHELTER PAD = 2.0% MAX SLOPE ALL DIRECTIONS

BUS HEADING SOUTHEAST
BUS STOP SIGN ID 925

REV.	DESCRIPTION	APPR.	DATE

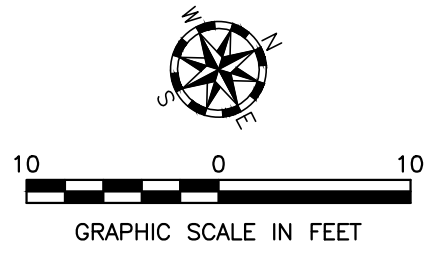


Drawn By	: V.M.
Checked By	: R.T.P.
Approved By	: R.T.P.
Project No.	: 5740102
Scale	: AS NOTED
Date	: 10/04/2016
Revision	: 0

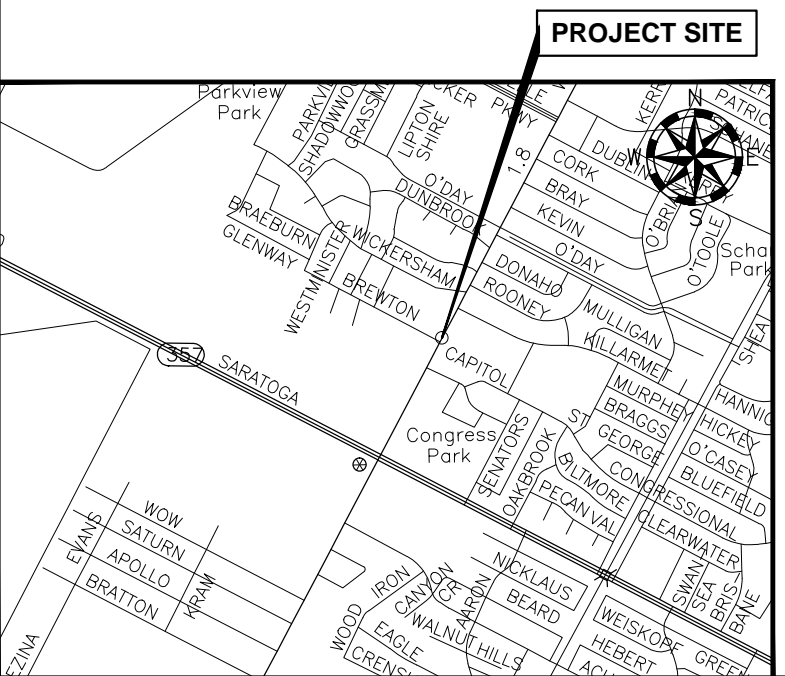
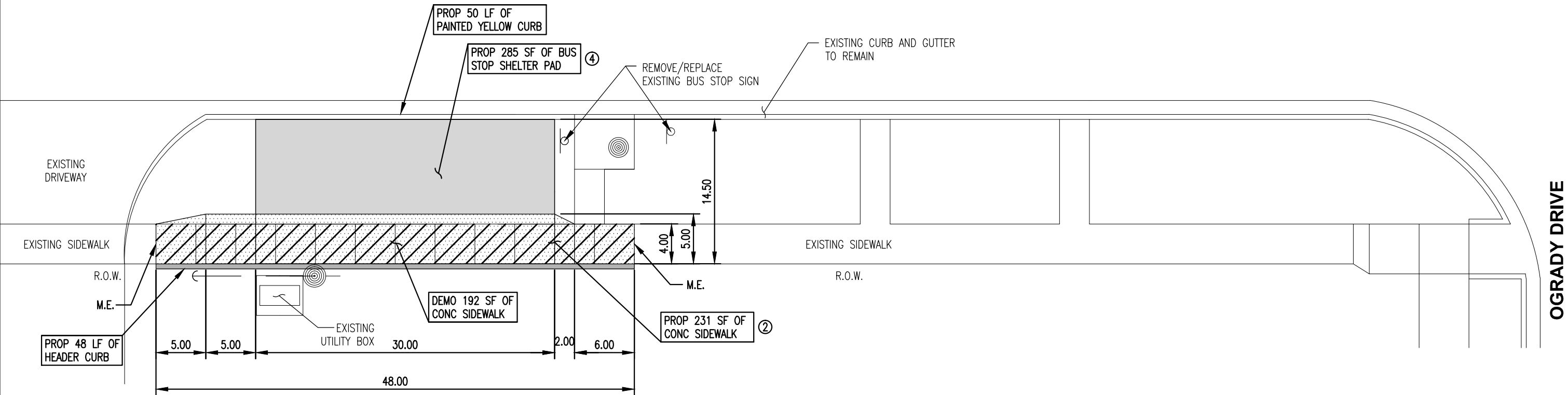
mgm
6000 S. Staples
Suite 207
Corpus Christi
Tx. 78413
P: 361-814-3070
Fax: 888-653-5510
Martinez, Guy & Maybik, Inc.
Civil / Structural Engineering & Surveying Services
TX. Bd. P.E. Firm Reg. No. F-1415

B
CORPUS CHRISTI REGIONAL
TRANSPORTATION AUTHORITY

**ADA BUS STOP IMPROVEMENTS
PHASE VI**
BUS STOP 925 - MCARDLE AT NESBIT
REGIONAL TRANSPORTATION AUTHORITY
CORPUS CHRISTI, TEXAS



FM 43 (WEBER)



NEW CONCRETE SHELTER PAD	285	SF
NEW SIDEWALK SECTION	231	SF
DEMOLITION OF EXISTING SIDEWALK	192	SF
SAW CUT CONCRETE PAVEMENT (FULL DEPTH)	8	LF
6" HEADER CURB	48	LF
BUS STOP CURB YELLOW STRIPING	50	LF

NOTES:

M.E. = MATCH EXISTING ELEVATION

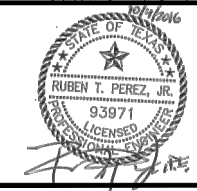
CROSS SLOPES FOR ALL CURB RAMPS, LANDINGS, SIDEWALKS, AND BUS STOP SHELTER PADS SHALL BE A MAXIMUM OF 2.0%

KEY NOTES:

- ① CONCRETE CURB RAMPS = 8.3% MAX SLOPE
- ② CONCRETE SIDEWALKS = 5.0% MAX SLOPE
- ③ CONCRETE LANDINGS = 2.0% MAX SLOPE ALL DIRECTIONS
- ④ CONCRETE BUS STOP = 2.0% MAX SLOPE ALL DIRECTIONS SHELTER PAD

EXHIBIT B – LOCATION 1
BUS HEADING NORTHEAST
BUS STOP SIGN ID 1026

REV.	DESCRIPTION	APPR.	DATE



Drawn By	: V.M.
Checked By	: R.T.P.
Approved By	: R.T.P.
Project No.	: 5740102
Scale	: AS NOTED
Date	: 10/04/2016
Revision	: 0



6000 S. Staples
Suite 207
Corpus Christi
Tx. 78413
P: 361-814-3070
Fax: 888-653-5510

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**CORPUS CHRISTI REGIONAL
TRANSPORTATION AUTHORITY**



**ADA BUS STOP IMPROVEMENTS
PHASE VI**

BUS STOP 1026 - FM 43 (WEBER) AT OGRADY

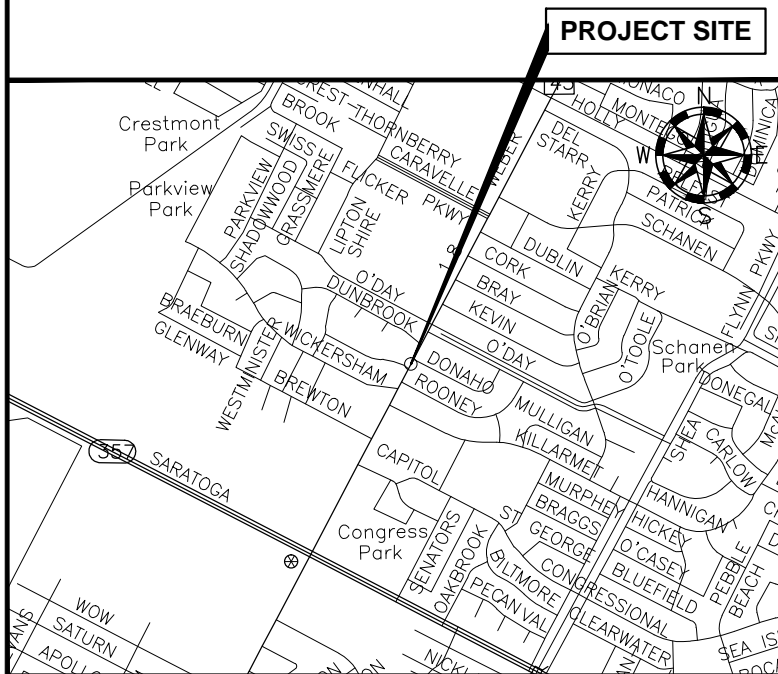
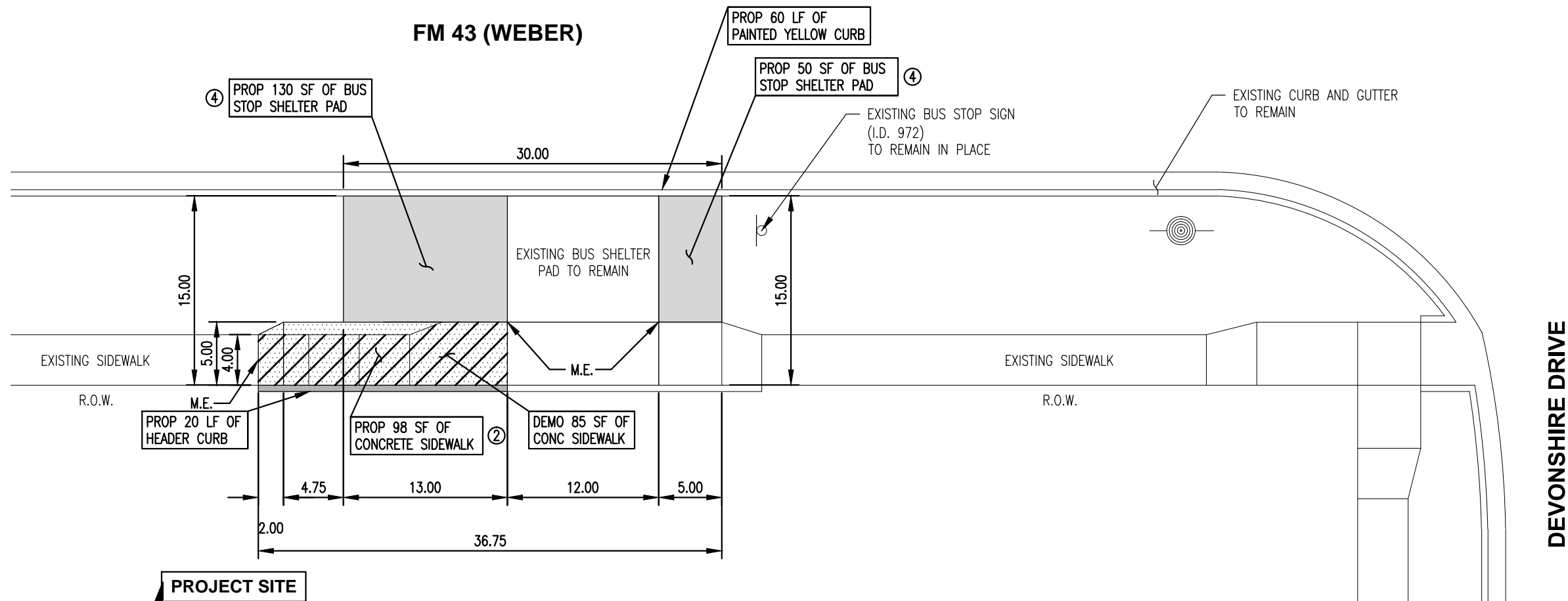
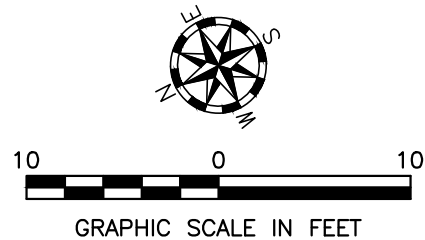
REGIONAL TRANSPORTATION AUTHORITY

CORPUS CHRISTI, TEXAS

Dwg. File:

30

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NEW CONCRETE SHELTER PAD	180	SF
NEW SIDEWALK SECTION	98	SF
DEMOLITION CONCRETE SIDEWALK	85	SF
SAW CUT CONCRETE PAVEMENT (FULL DEPTH)	9	LF
6" HEADER CURB	20	LF
BUS STOP YELLOW STRIPING	60	LF

NOTES:

M.E. = MATCH EXISTING ELEVATION

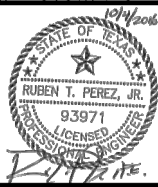
CROSS SLOPES FOR ALL CURB RAMPS, LANDINGS, SIDEWALKS, AND BUS STOP SHELTER PADS SHALL BE A MAXIMUM OF 2.0%

KEY NOTES:

- ① CONCRETE CURB RAMPS = 8.3% MAX SLOPE
- ② CONCRETE SIDEWALKS = 5.0% MAX SLOPE
- ③ CONCRETE LANDINGS = 2.0% MAX SLOPE ALL DIRECTIONS
- ④ CONCRETE BUS STOP = 2.0% MAX SLOPE ALL DIRECTIONS SHELTER PAD

EXHIBIT B - LOCATION 2
BUS HEADING SOUTHWEST
BUS STOP SIGN ID 972

REV.	DESCRIPTION	APPR.	DATE



Drawn By	: V.M.
Checked By	: R.T.P.
Approved By	: R.T.P.
Project No.	: 5740102
Scale	: AS NOTED
Date	: 10/04/2016
Revision	: 0



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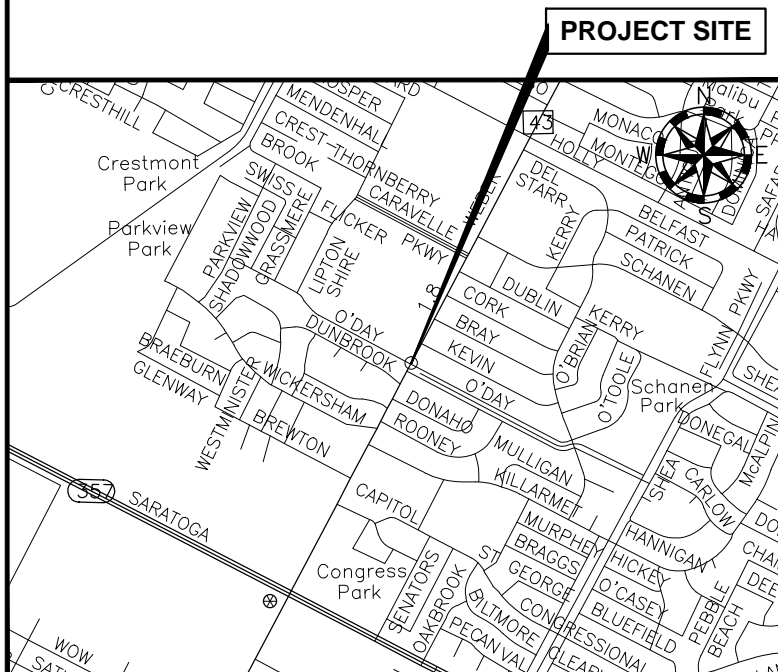
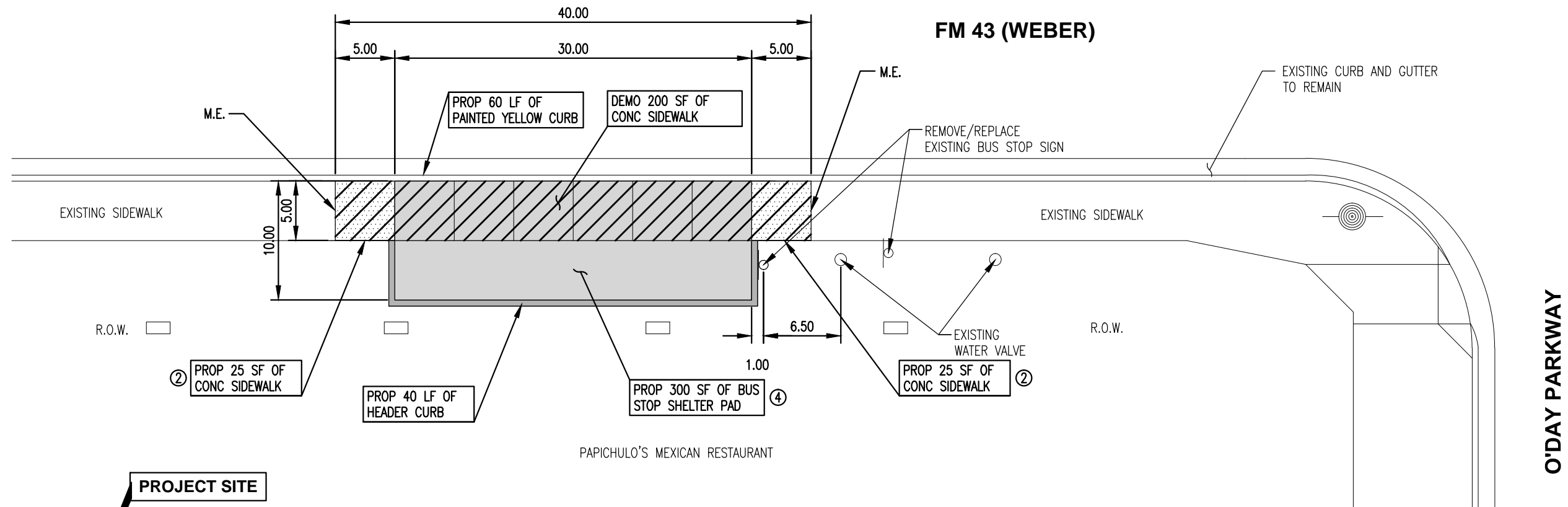
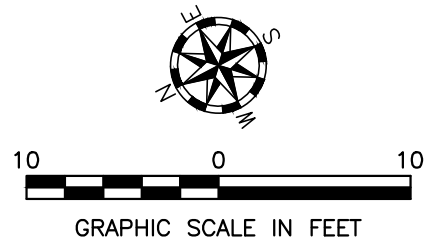
CORPUS CHRISTI REGIONAL
TRANSPORTATION AUTHORITY

ADA BUS STOP IMPROVEMENTS
PHASE VI
BUS STOP 972 - FM 43 (WEBER) AT ROONEY
REGIONAL TRANSPORTATION AUTHORITY
CORPUS CHRISTI, TEXAS

Dwg. File:

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NEW CONCRETE SHELTER PAD	300	SF
NEW SIDEWALK SECTION	50	SF
DEMOLITION OF EXISTING SIDEWALK	200	SF
SAW CUT CONCRETE PAVEMENT (FULL DEPTH)	10	LF
RELOCATE EXISTING BUS STOP SIGN	1	EA
6" HEADER CURB	40	LF
BUS STOP CURB YELLOW STRIPING	60	LF

NOTES:

M.E. = MATCH EXISTING ELEVATION

CROSS SLOPES FOR ALL CURB RAMPS, LANDINGS, SIDEWALKS, AND BUS STOP SHELTER PADS SHALL BE A MAXIMUM OF 2.0%

KEY NOTES:

- ① CONCRETE CURB RAMPS = 8.3% MAX SLOPE
- ② CONCRETE SIDEWALKS = 5.0% MAX SLOPE
- ③ CONCRETE LANDINGS = 2.0% MAX SLOPE ALL DIRECTIONS
- ④ CONCRETE BUS STOP SHELTER PAD = 2.0% MAX SLOPE ALL DIRECTIONS

EXHIBIT B - LOCATION 3
BUS HEADING SOUTHWEST
BUS STOP SIGN ID 971

REV.	DESCRIPTION	APPR.	DATE

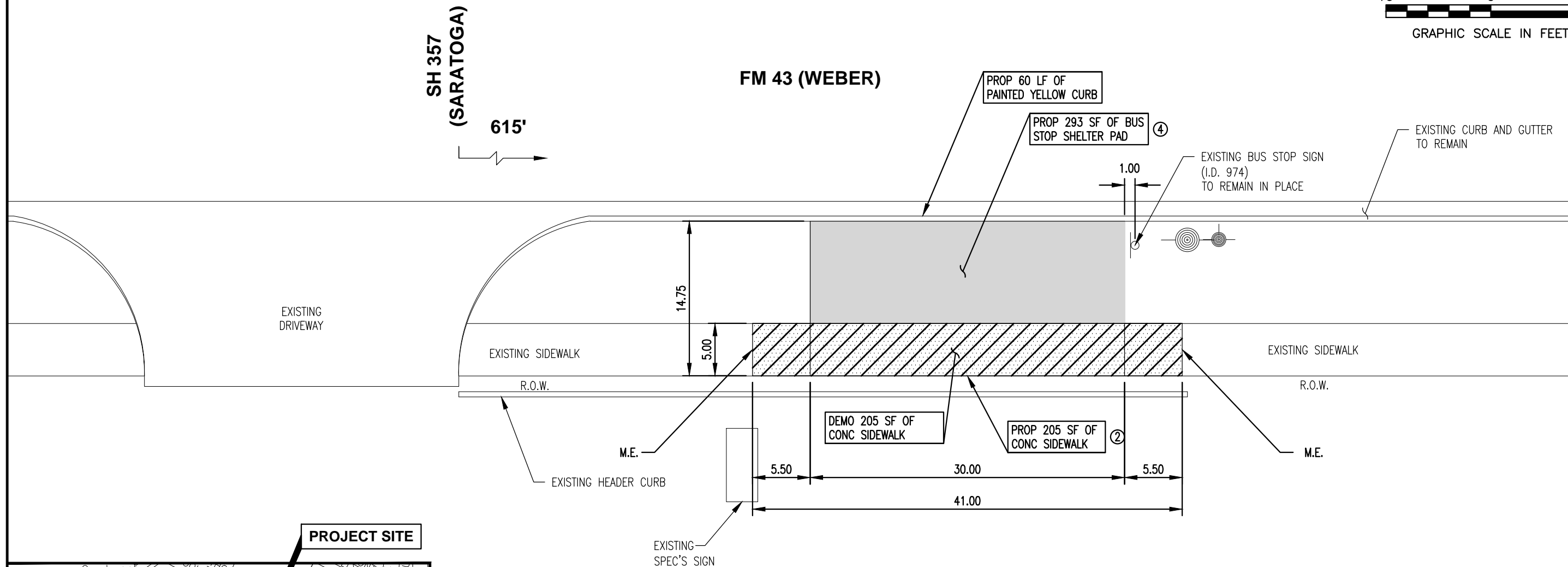
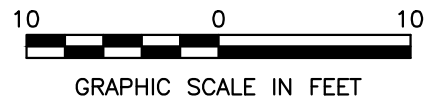


Drawn By	: V.M.
Checked By	: R.T.P.
Approved By	: R.T.P.
Project No.	: 5740102
Scale	: AS NOTED
Date	: 10/04/2016
Revision	: 0

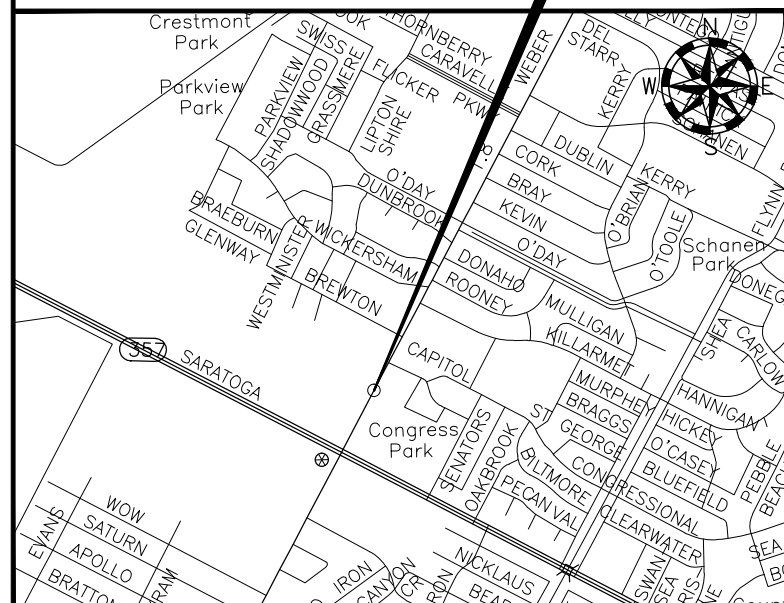
mgm
6000 S. Staples
Suite 207
Corpus Christi
TX 78413
P: 361-814-3070
Fax: 888-653-5510
Martinez, Guy & Maybik, Inc.
Civil / Structural Engineering & Surveying Services
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CORPUS CHRISTI REGIONAL
TRANSPORTATION AUTHORITY

ADA BUS STOP IMPROVEMENTS
PHASE VI
BUS STOP 971 - FM 43 (WEBER) AT O'DAY
REGIONAL TRANSPORTATION AUTHORITY
CORPUS CHRISTI, TEXAS



PROJECT SITE



NEW CONCRETE SHELTER PAD	293	SF
NEW SIDEWALK SECTION	205	SF
DEMOLITION OF EXISTING SIDEWALK	205	SF
SAW CUT CONCRETE PAVEMENT (FULL DEPTH)	10	LF
BUS STOP CURB YELLOW STRIPING	60	LF

NOTES:

M.E. = MATCH EXISTING ELEVATION

CROSS SLOPES FOR ALL CURB RAMPS, LANDINGS, SIDEWALKS, AND BUS STOP SHELTER PADS SHALL BE A MAXIMUM OF 2.0%

KEY NOTES:

- ① CONCRETE CURB RAMPS = 8.3% MAX SLOPE
- ② CONCRETE SIDEWALKS = 5.0% MAX SLOPE
- ③ CONCRETE LANDINGS = 2.0% MAX SLOPE ALL DIRECTIONS
- ④ CONCRETE BUS STOP = 2.0% MAX SLOPE ALL DIRECTIONS SHELTER PAD

EXHIBIT B – LOCATION 4
BUS HEADING SOUTHWEST
BUS STOP SIGN ID 974

REV.	DESCRIPTION	APPR.	DATE



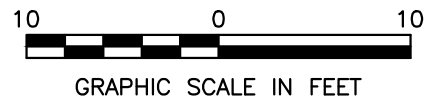
Drawn By	: V.M.
Checked By	: R.T.P.
Approved By	: R.T.P.
Project No.	: 5740102
Scale	: AS NOTED
Date	: 10/04/2016
Revision	: 0

mgm
6000 S. Staples
Suite 207
Corpus Christi
Tx. 78413
P: 361-814-3070
Fax: 888-653-5510
Martinez, Guy & Maybik, Inc.
Civil / Structural Engineering & Surveying Services
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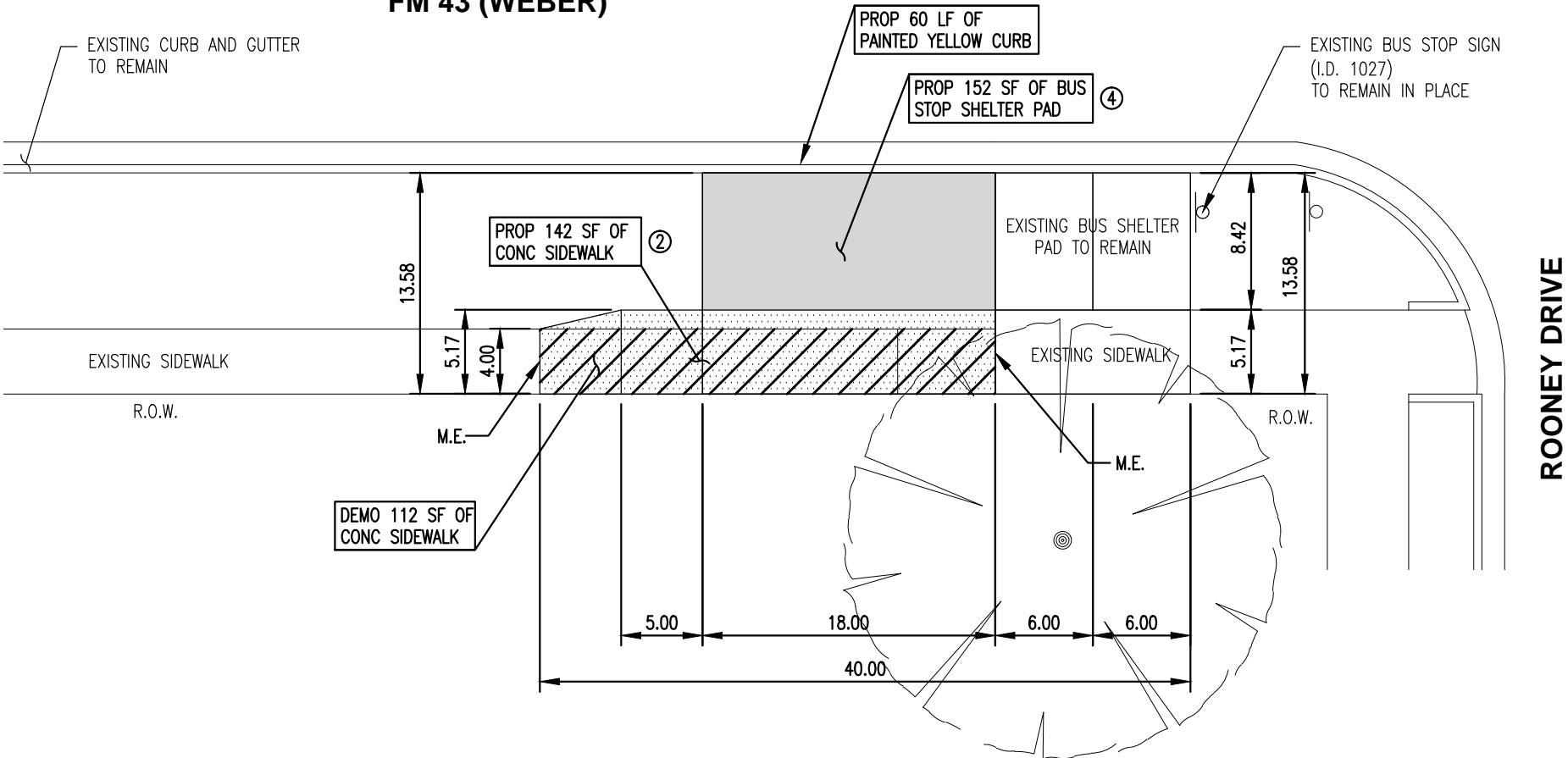
B
CORPUS CHRISTI REGIONAL
TRANSPORTATION AUTHORITY

**ADA BUS STOP IMPROVEMENTS
PHASE VI
BUS STOP 974 -FM 43 (WEBER) AT SH 357 (SARATOGA)**
REGIONAL TRANSPORTATION AUTHORITY
CORPUS CHRISTI, TEXAS

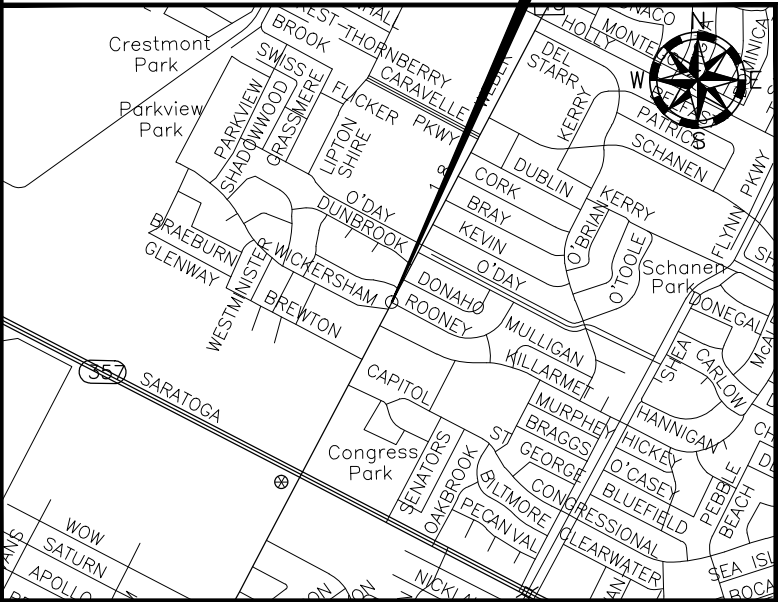
Dwg. File:
33
Sheet 33 of 44



FM 43 (WEBER)



PROJECT SITE



NEW CONCRETE SHELTER PAD	152	SF
NEW SIDEWALK SECTION	142	SF
DEMOLITION OF EXISTING SIDEWALK	112	SF
SAW CUT CONCRETE PAVEMENT (FULL DEPTH)	8	LF
BUS STOP CURB YELLOW STRIPING	60	LF

NOTES:

M.E. = MATCH EXISTING ELEVATION

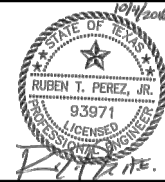
CROSS SLOPES FOR ALL CURB RAMPS, LANDINGS, SIDEWALKS, AND BUS STOP SHELTER PADS SHALL BE A MAXIMUM OF 2.0%

KEY NOTES:

- ① CONCRETE CURB RAMPS = 8.3% MAX SLOPE
- ② CONCRETE SIDEWALKS = 5.0% MAX SLOPE
- ③ CONCRETE LANDINGS = 2.0% MAX SLOPE ALL DIRECTIONS
- ④ CONCRETE BUS STOP = 2.0% MAX SLOPE ALL DIRECTIONS SHELTER PAD

EXHIBIT B – LOCATION 5
BUS HEADING SOUTHWEST
BUS STOP SIGN ID 1027

REV.	DESCRIPTION	APPR.	DATE



Drawn By	: V.M.
Checked By	: R.T.P.
Approved By	: R.T.P.
Project No.	: 5740102
Scale	: AS NOTED
Date	: 10/04/2016
Revision	: 0



6000 S. Staples
Suite 207
Corpus Christi
Tx. 78413
P: 361-814-3070
Fax: 888-653-5510

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Civil / Structural Engineering & Surveying Services
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**CORPUS CHRISTI REGIONAL
TRANSPORTATION AUTHORITY**

**ADA BUS STOP IMPROVEMENTS
PHASE VI**

BUS STOP 1027 - FM 43 (WEBER) AT ROONEY

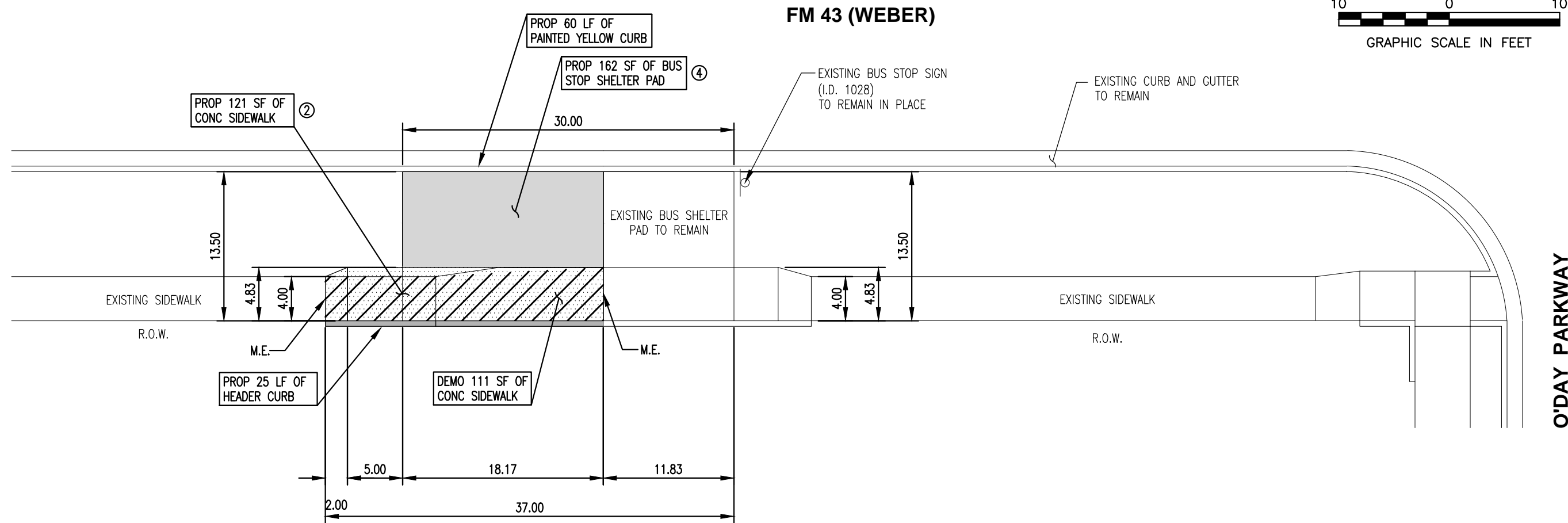
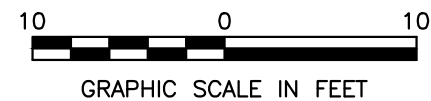
REGIONAL TRANSPORTATION AUTHORITY

CORPUS CHRISTI, TEXAS

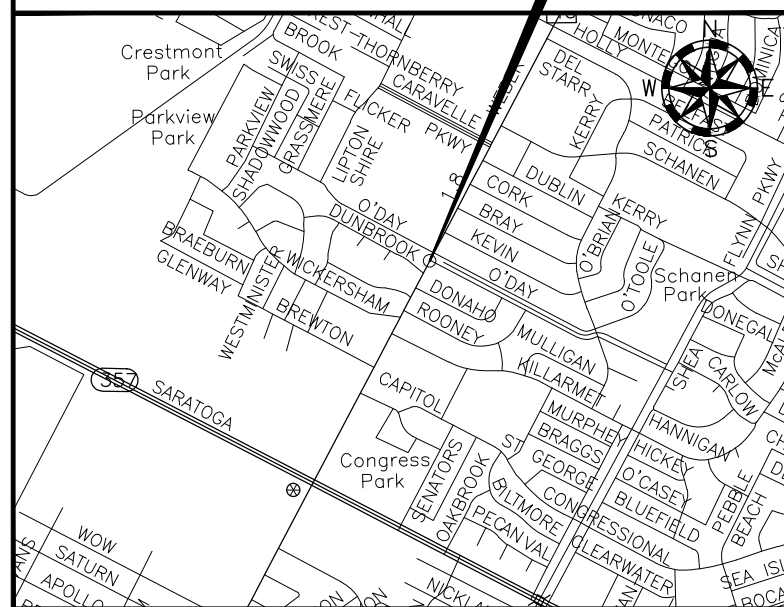
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34

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PROJECT SITE



NEW CONCRETE SHELTER PAD	162	SF
NEW SIDEWALK SECTION	121	SF
DEMOLITION OF EXISTING SIDEWALK	111	SF
SAW CUT CONCRETE PAVEMENT (FULL DEPTH)	9	LF
6" HEADER CURB	25	LF
BUS STOP CURB YELLOW STRIPING	60	LF

NOTES:

M.E. = MATCH EXISTING ELEVATION

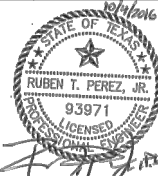
CROSS SLOPES FOR ALL CURB RAMPS, LANDINGS, SIDEWALKS, AND BUS STOP SHELTER PADS SHALL BE A MAXIMUM OF 2.0%

KEY NOTES:

- ① CONCRETE CURB RAMPS = 8.3% MAX SLOPE
- ② CONCRETE SIDEWALKS = 5.0% MAX SLOPE
- ③ CONCRETE LANDINGS = 2.0% MAX SLOPE ALL DIRECTIONS
- ④ CONCRETE BUS STOP = 2.0% MAX SLOPE ALL DIRECTIONS SHELTER PAD

EXHIBIT B – LOCATION 6
BUS HEADING NORTHEAST
BUS STOP SIGN ID 1028

REV.	DESCRIPTION	APPR.	DATE



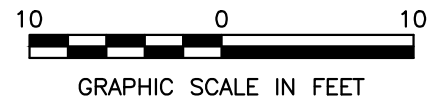
Drawn By	: V.M.
Checked By	: R.T.P.
Approved By	: R.T.P.
Project No.	: 5740102
Scale	: AS NOTED
Date	: 10/04/2016
Revision	: 0

mgm
6000 S. Staples
Suite 207
Corpus Christi
TX 78413
P: 361-814-3070
Fax: 888-653-5510
Martinez, Guy & Maybik, Inc.
Civil / Structural Engineering & Surveying Services
TX. Bd. P.E. Firm Reg. No. F-1415



CORPUS CHRISTI REGIONAL
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**ADA BUS STOP IMPROVEMENTS
PHASE VI**
BUS STOP 1028 - FM 43 (WEBER) AT O'DAY
REGIONAL TRANSPORTATION AUTHORITY
CORPUS CHRISTI, TEXAS



FM 43 (WEBER)

PANAMA DRIVE

63'

EXISTING CURB AND GUTTER
TO REMAIN

PROP 60 LF OF
PAINTED YELLOW CURB

PROP 349 SF OF BUS
STOP SHELTER PAD ④

EXISTING BUS STOP SIGN
(I.D. 1035)
TO REMAIN IN PLACE

24.42

PROP 163 SF OF
CONC SIDEWALK ②

EXISTING BUS SHELTER
PAD TO REMAIN

PROP 225 SF OF
CONC SIDEWALK ②

EXISTING SIDEWALK

R.O.W.

M.E.

DEMO 141 SF OF
CONC SIDEWALK

15.00

PROP 18 LF OF
HEADER CURB

18.00

M.E. R.O.W.

12.00

M.E.

6.00
96.00

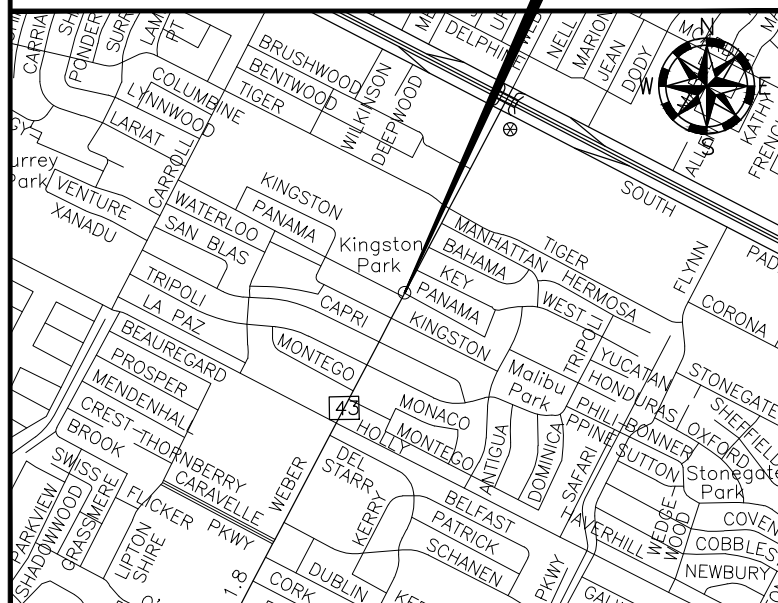
DEMO 185 SF OF
CONC SIDEWALK

45.00

EXISTING SIDEWALK

M.E. R.O.W.

PROJECT SITE



NEW CONCRETE SHELTER PAD	349	SF
NEW SIDEWALK SECTION	388	SF
DEMOLITION OF EXISTING SIDEWALK	326	SF
SAW CUT CONCRETE PAVEMENT (FULL DEPTH)	19	LF
6" HEADER CURB	18	LF
BUS STOP CURB YELLOW STRIPING	60	LF

NOTES:

M.E. = MATCH EXISTING ELEVATION

CROSS SLOPES FOR ALL CURB RAMPS,
LANDINGS, SIDEWALKS, AND BUS STOP SHELTER
PADS SHALL BE A MAXIMUM OF 2.0%

KEY NOTES:

- ① CONCRETE CURB RAMPS = 8.3% MAX SLOPE
- ② CONCRETE SIDEWALKS = 5.0% MAX SLOPE
- ③ CONCRETE LANDINGS = 2.0% MAX SLOPE ALL DIRECTIONS
- ④ CONCRETE BUS STOP = 2.0% MAX SLOPE ALL DIRECTIONS
SHELTER PAD

EXHIBIT B - LOCATION 7
BUS HEADING NORTHEAST
BUS STOP SIGN ID 1035

REV.	DESCRIPTION	APPR.	DATE



Drawn By	: V.M.
Checked By	: R.T.P.
Approved By	: R.T.P.
Project No.	: 5740102
Scale	: AS NOTED
Date	: 10/04/2016
Revision	: 0

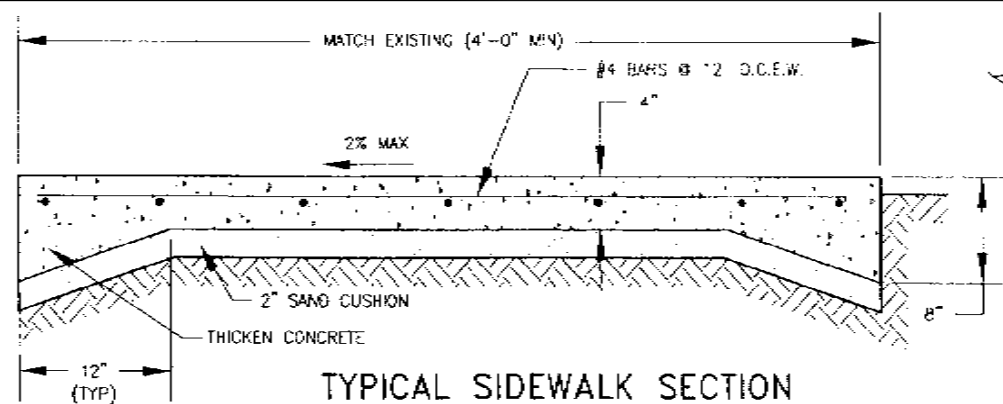
mgm
6000 S. Staples
Suite 207
Corpus Christi
Tx. 78413
P: 361-814-3070
Fax: 888-653-5510
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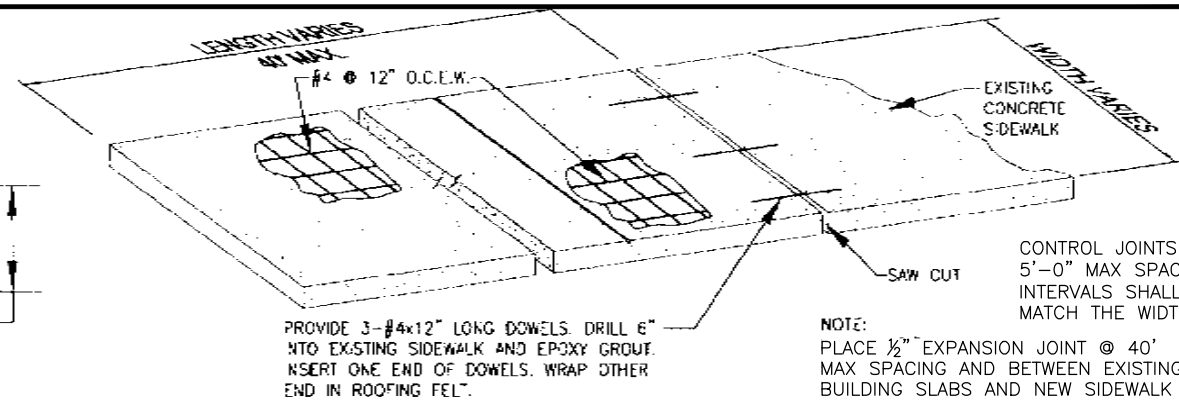
CORPUS CHRISTI REGIONAL
TRANSPORTATION AUTHORITY

**ADA BUS STOP IMPROVEMENTS
PHASE VI**
BUS STOP 1035 - FM 43 (WEBER) AT PANAMA
REGIONAL TRANSPORTATION AUTHORITY
CORPUS CHRISTI, TEXAS

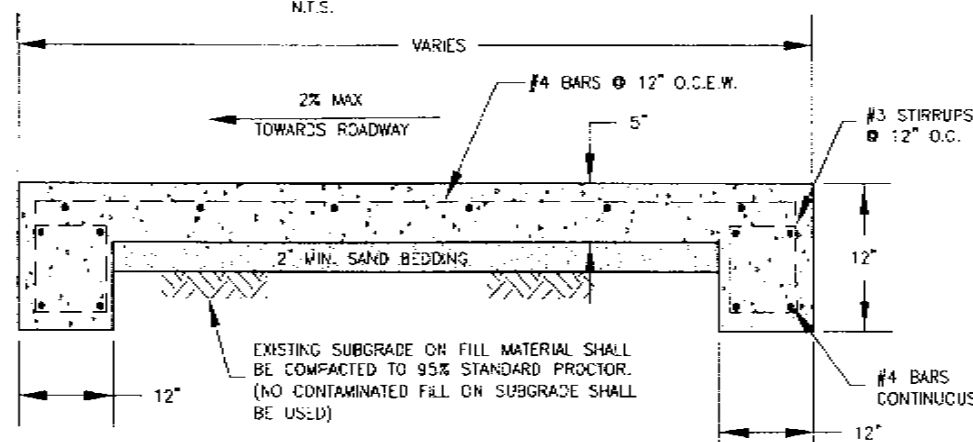
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36
Sheet 36 of 44



TYPICAL SIDEWALK SECTION

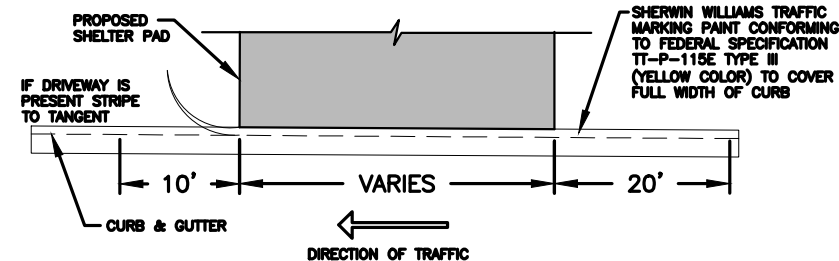


SIDEWALK SECTION

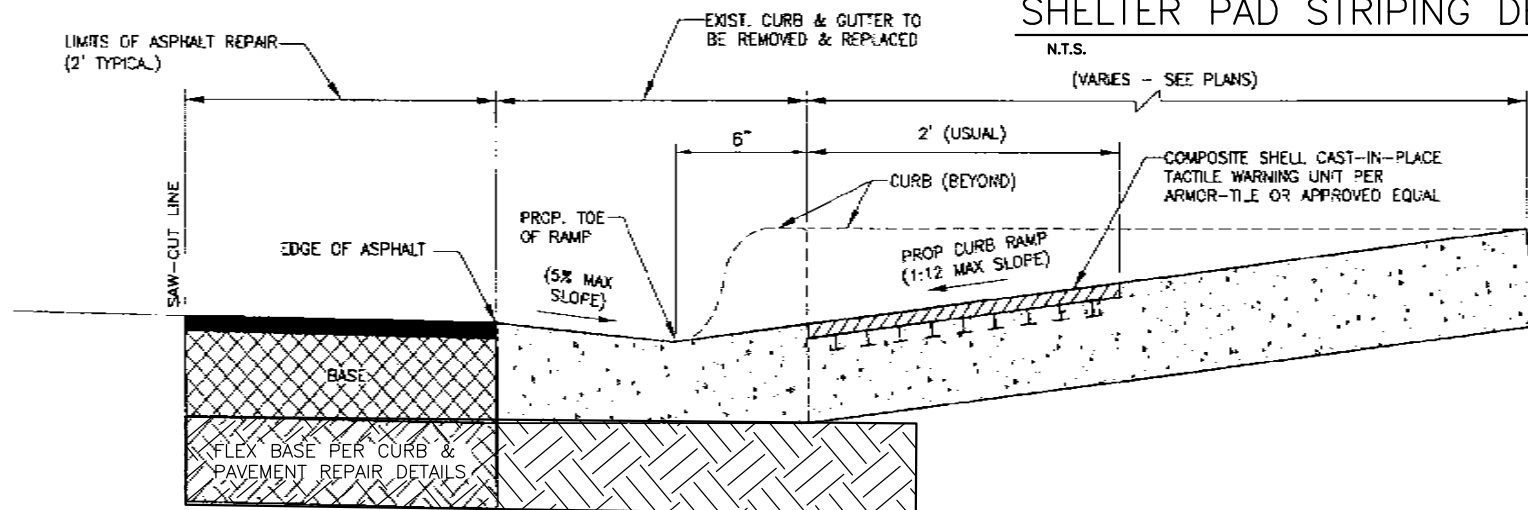


TYPICAL SHELTER PAD SECTION

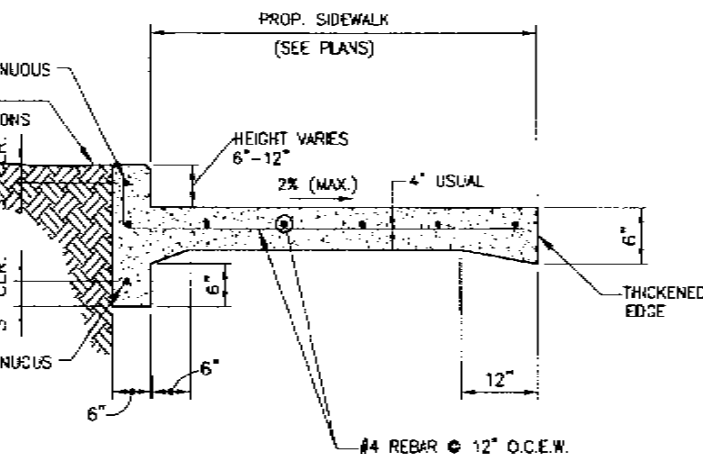
- NOTE: (1) WHERE SHELTER PAD IS BEING INSTALLED ADJACENT TO EXISTING CURB AND GUTTER SECTION, DRILL 6" x 5/8" DIA. HOLE INTO EXISTING CONCRETE CURB. SLUSH HOLE W/ EPOXY, INSERT 18" x 1/2" DIA. SMOOTH BARS AT 24" O.C. LEAVE EXPOSED REINFORCEMENT W/ PLASTIC SLEEVE TO PREVENT BONDING. (2) WHERE SHELTER PAD IS BEING INSTALLED AND EXISTING CURB AND GUTTER SECTION REPLACED, REFER TO 5" 'L' CURB CURB SECTION DETAIL. (3) PLACE EXPANSION MATERIAL BETWEEN SHELTER PAD AND EXISTING CURB & GUTTER/SIDEWALK. (4) DOWEL SHELTER PAD INTO EXISTING SIDEWALK.



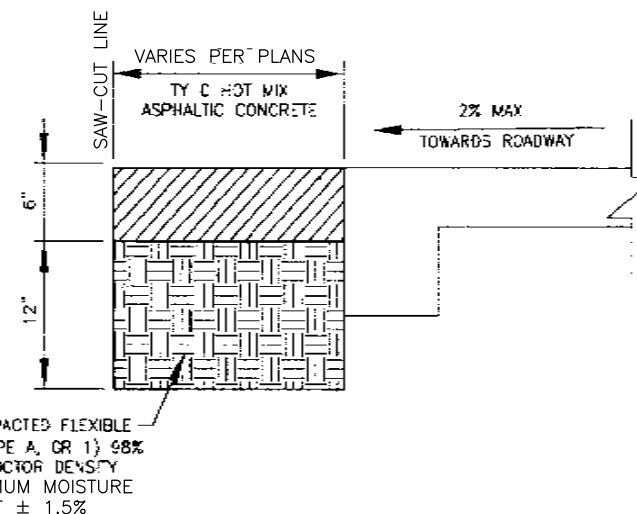
SHELTER PAD STRIPING DETAIL



CURB RAMP AND PAVMT. REPAIR DETAIL



SIDEWALK WITH RETAINING CURB (HEIGHT >6" TO 12")



PAVEMENT REPAIR DETAIL AT SHELTER PAD

REV.	DESCRIPTION	APPR.	DATE



Drawn By	
Checked By	R.T.P.
Approved By	R.T.P.
Project No.	5740102
Scale	AS NOTED
Date	10/04/2016
Revision	0



6000 S. Staples
Suite 207
Corpus Christi
TX 78413
P: 361-814-3070
Fax: 888-653-5510
Martinez, Guy & Maybik, Inc.
Civil / Structural Engineering & Surveying Services
TX, Bd. P.E. Firm Reg. No. F-1415



CORPUS CHRISTI REGIONAL
TRANSPORTATION AUTHORITY

ADA BUS STOP IMPROVEMENTS
PHASE VI
MISCELLANEOUS DETAILS
REGIONAL TRANSPORTATION AUTHORITY
CORPUS CHRISTI, TEXAS

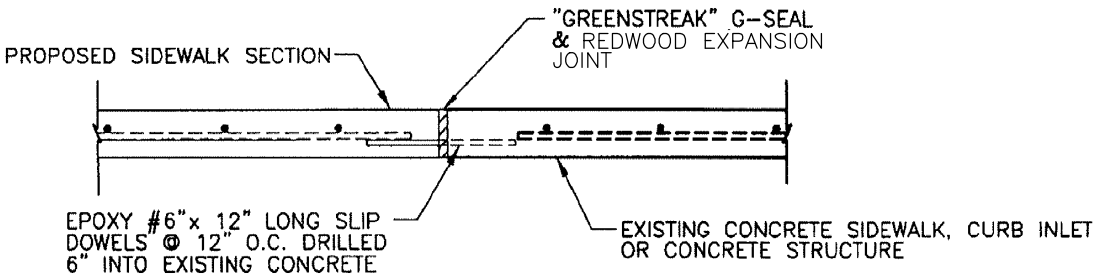
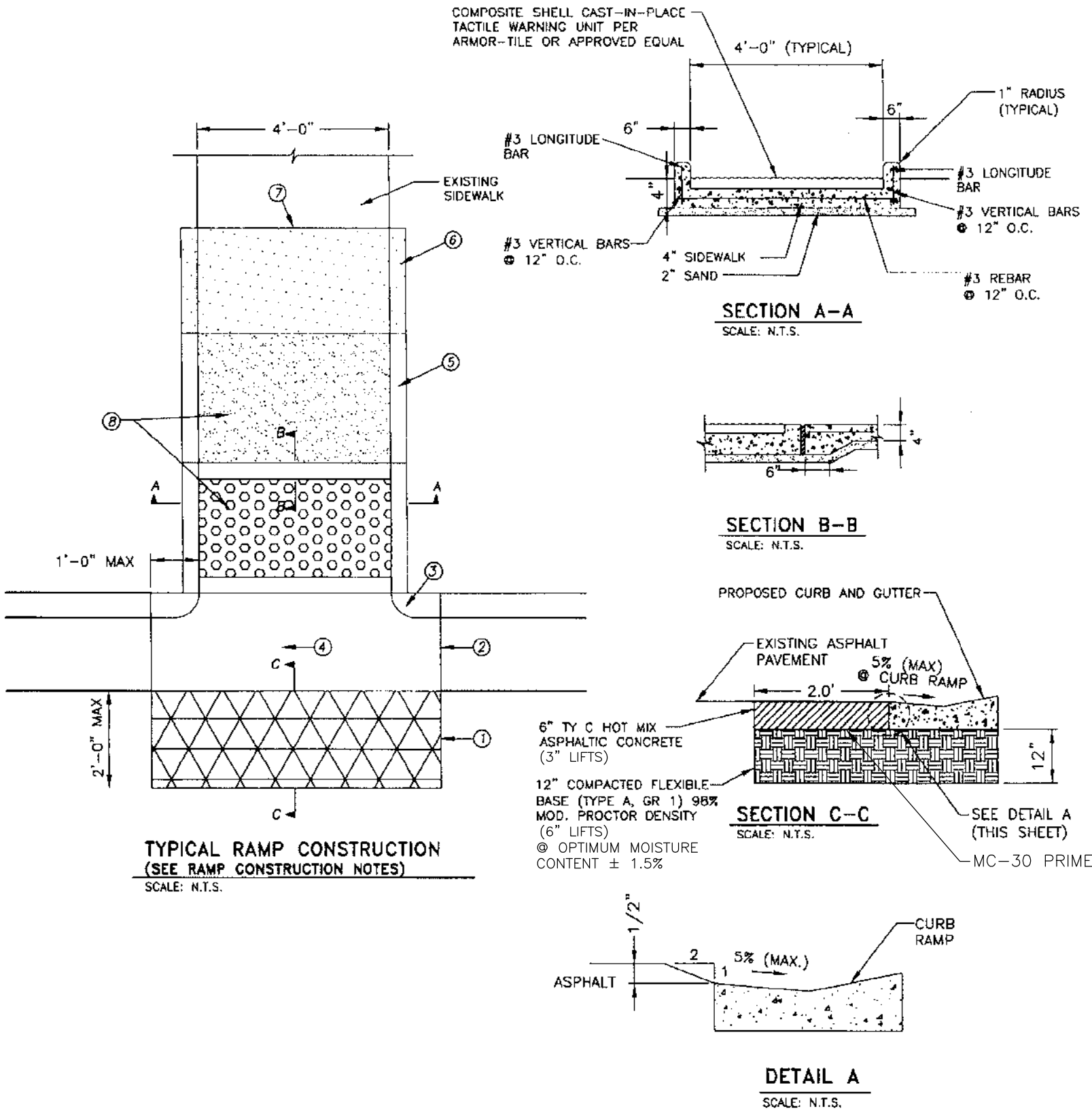
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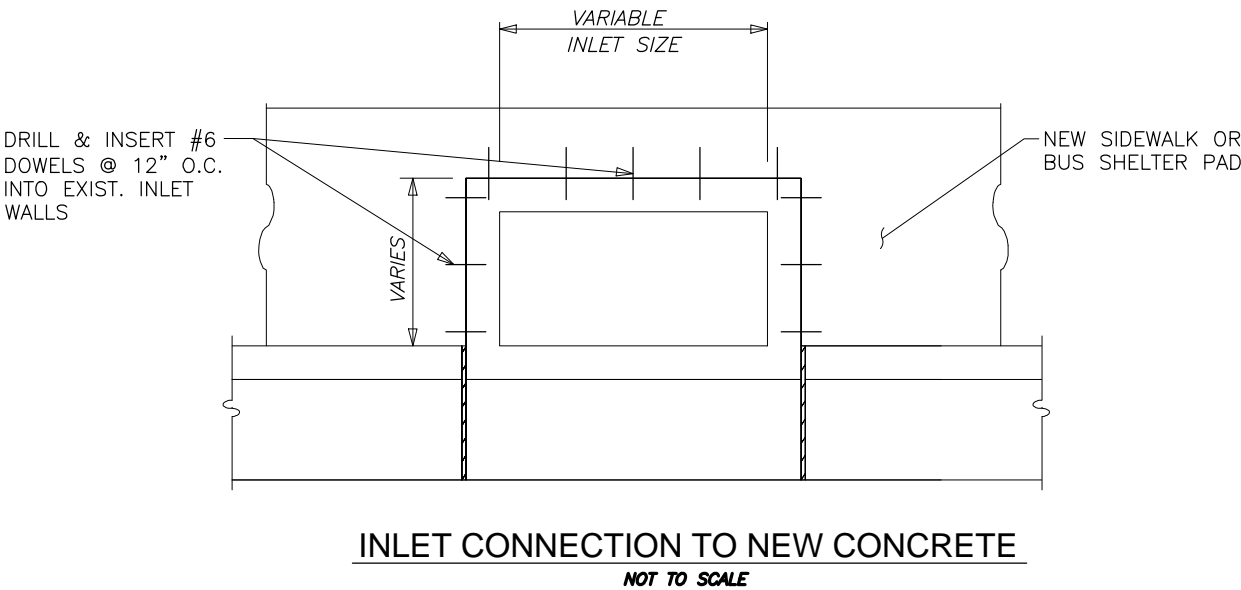
Sheet 37 of 44

RAMP CONSTRUCTION NOTES

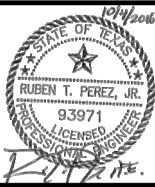
- 1. THE EXISTING PAVEMENT SHALL BE SAW CUT. SEE SECTION C-C ON THIS SHEET FOR THE MINIMUM PAVEMENT REPAIR REQUIREMENTS.
- 2. THE EXISTING CURB AND GUTTER SHALL BE SAW CUT TO FULL DEPTH. THE SAW CUT MAY BE ELIMINATED ONLY IF AN EXISTING EXPANSION JOINT IS LOCATED WITHIN 1' OF THE PROPOSED CUT LOCATION.
- 3. THE EXISTING CURB SHALL HAVE A SMOOTH TRANSITION BACK TO THE 4" HEADER CURB. VERTICAL REBAR SHALL BE PLACED IN THE TRANSITION CURB.
- 4. MAINTAIN THE GRADE OF THE GUTTER FLOW LINE. IF PONDING IS ALLOWED BY THE CONTRACTOR, HE WILL BE REQUIRED TO RECONSTRUCT AT HIS COST.
- 5. 4" HEADER CURB. IF THE NATURAL GROUND IS LESS THAN 2" ABOVE THE TOP OF SIDEWALK, THE HEADER CURB MAY BE ELIMINATED AND THE GROUND GRADED ABACK AT A 2:1 SLOPE.
- 6. FOR LONG RUNS, THE LAST 2' SECTION SHALL BE FADED OUT FROM 4" TO 0".
- 7. EXISTING SIDEWALK SHALL BE SAW CUT FULL DEPTH. THE SAW CUT MAY BE ELIMINATED ONLY IF AN EXISTING EXPANSION JOINT IS LOCATED WITHIN 1' OF THE PROPOSED CUT LOCATION.
- 8. THE EXACT DIMENSIONS FOR THE RAMPS WILL VARY BASED ON LOCATION AND FIELD CONDITIONS. CONTRACTOR WILL VERIFY THAT THE MINIMUM ADA STANDARDS ARE COMPLIED WITH.



CONNECTIONS DETAIL
(FOR ALL CONNECTION TO EXISTING CONCRETE)
SCALE: N.T.S.



REV.	DESCRIPTION	APPR.	DATE



Drawn By	:	
Checked By	:	R.T.P.
Approved By	:	R.T.P.
Project No.	:	5740102
Scale	:	AS NOTED
Date	:	10/04/2016
Revision	:	0



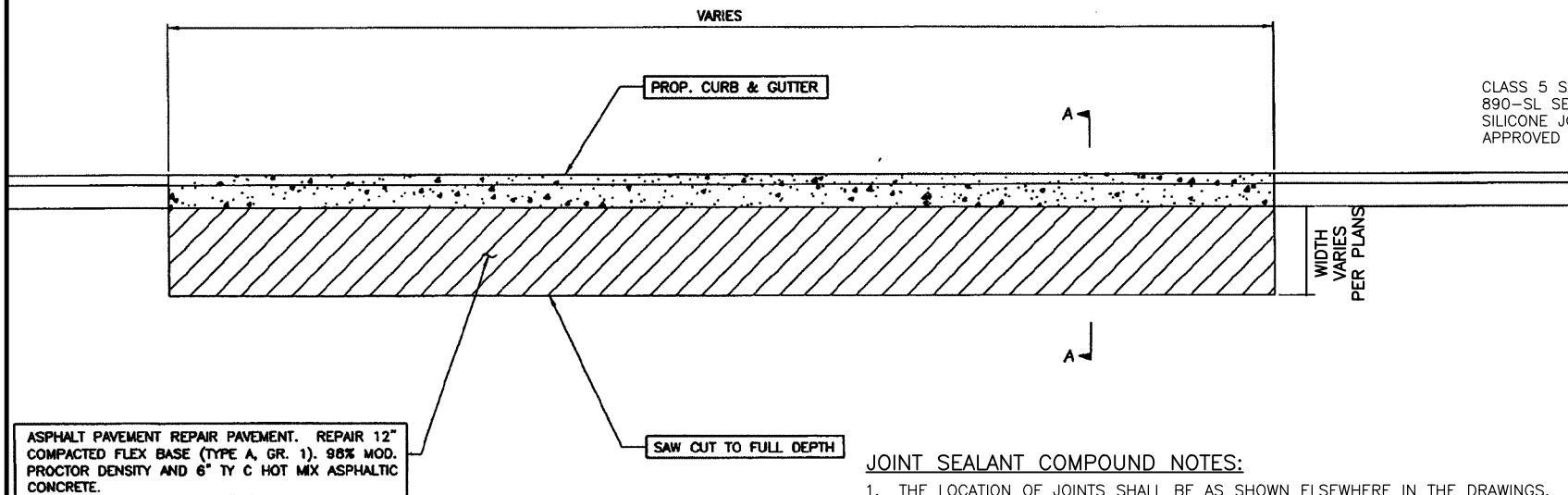
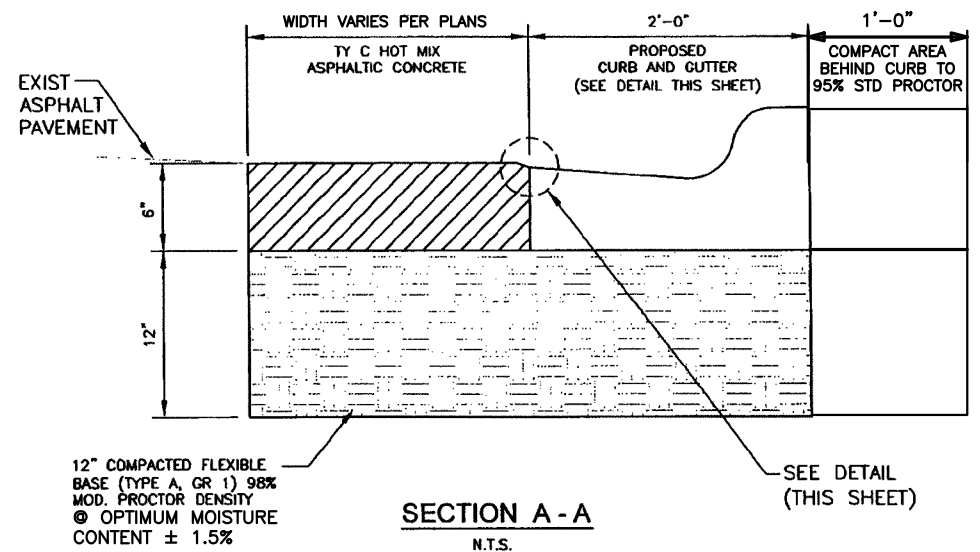
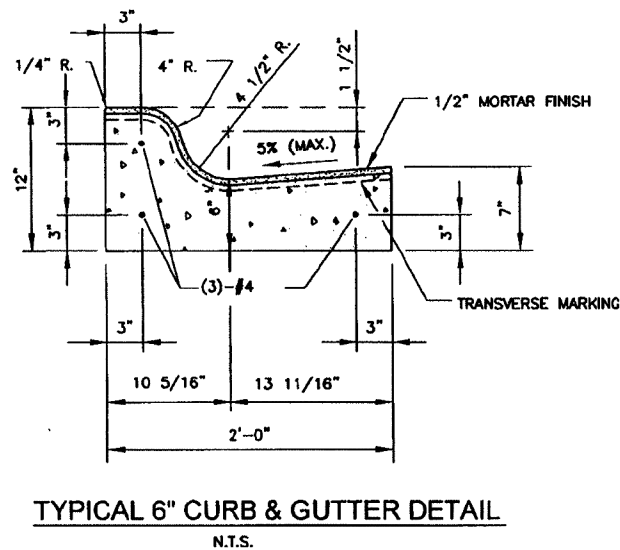
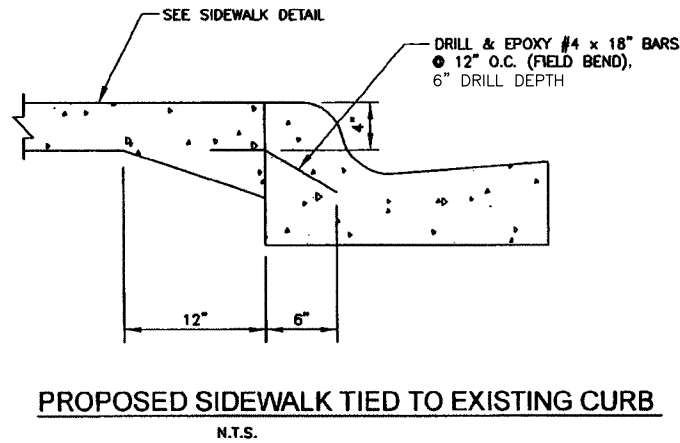
Martinez, Guy & Maybik, Inc.
Civil / Structural Engineering & Surveying Services
TX. Bd. P.E. Firm Reg. No. F-1415

6000 S. Staples
Suite 207
Corpus Christi
TX 78413
P: 361-814-3070
Fax: 888-653-5510

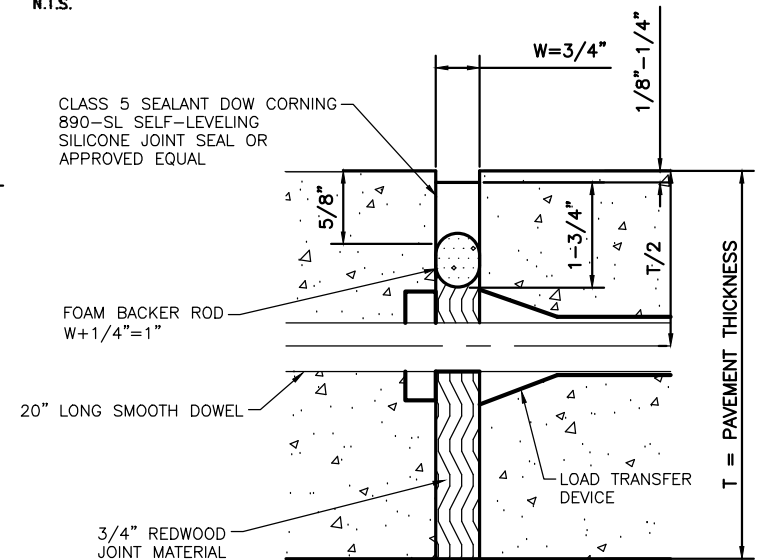
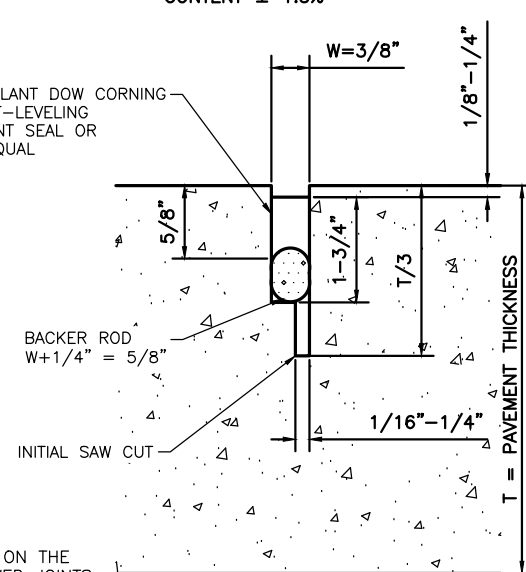


CORPUS CHRISTI REGIONAL
TRANSPORTATION AUTHORITY

ADA BUS STOP IMPROVEMENTS
PHASE VI
MISCELLANEOUS DETAILS
REGIONAL TRANSPORTATION AUTHORITY
CORPUS CHRISTI, TEXAS



CLASS 5 SEALANT DOW CORNING
890-SL SELF-LEVELING
SILICONE JOINT SEAL OR
APPROVED EQUAL

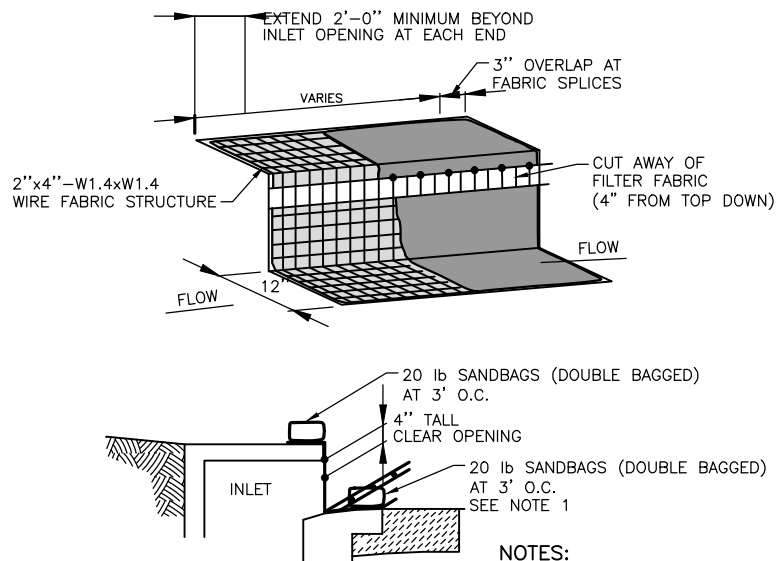


JOINT SEALANT COMPOUND NOTES:

1. THE LOCATION OF JOINTS SHALL BE AS SHOWN ELSEWHERE IN THE DRAWINGS.
2. THE JOINT RESERVOIR FOR SEALANT SHALL BE SAWED UNLESS OTHERWISE SHOWN ON THE PLANS FOR THE LONGITUDINAL AND TRANSVERSE CONSTRUCTION AND THE TWO SAWED JOINTS.
3. THE JOINTS SHALL BE CLEANED IN ACCORDANCE WITH THE SEALANT MANUFACTURE'S RECOMMENDATION. PRIOR TO BEGINNING OPERATIONS, THE CONTRACTOR SHALL SUBMIT A STATEMENT FROM THE SEALANT MANUFACTURER SHOWING THE RECOMMENDED EQUIPMENT AND INSTALLATION PROCEDURES TO BE USED.
4. THE SAW CUT FOR THE LONGITUDINAL JOINT SHALL BE ONE FOURTH THE SLAB THICKNESS WHEN CRUSHED LIMESTONE IS USED AS THE COARSE AGGREGATE.

CURB INLET PROTECTION NOTES:

1. TO HOLD THE FILTER DIKE IN PLACE, 20 LB SANDBAGS SHALL BE USED AT 3' O.C. WHERE MINIMUM CLEARANCES CAUSE TRAFFIC TO DRIVE IN THE GUTTER, THE CONTRACTOR MAY SUBSTITUTE A 1"x4" BOARD, SECURED WITH 1/4" OR 3/8" CONCRETE SCREWS. THE 1/4" OR 3/8" CONCRETE SCREWS SHALL BE ATTACHED TO THE GUTTER BY DRILLING AN APPROPRIATE PILOT HOLE WITH A CONCRETE BIT AND INSERT PLASTIC FASTENERS. THE TOP OF THE SCREW SHALL BE RECESSED BELOW THE TOP OF THE BOARD. THE SCREWS SHALL BE PLACED ON 3' O.C. THIS METHOD IS USED IN LIEU OF SANDBAGS, IN THE GUTTER ONLY, TO HOLD THE FILTER DIKE IN PLACE. UPON REMOVAL, EITHER LEAVE THE PLASTIC FASTENERS IN PLACE, OR REMOVE THE PLASTIC FASTENERS, CLEAN ANY DIRT/DEBRIS FROM THE SCREW LOCATIONS, APPLY CHEMICAL SANDING AGENT AND APPLY NON-SHRINK GROUT FLUSH WITH THE SURFACE OF THE GUTTER. THIS METHOD SHALL NOT BE USED ON THE INLET IN LIEU OF SANDBAGS.
2. A SECTION OF FILTER FABRIC SHALL BE REMOVED AS SHOWN ON THIS DETAIL OR AS DIRECTED BY THE ENGINEER OR DESIGNATED REPRESENTATIVE. FABRIC MUST BE SECURED TO WIRE BACKING WITH CLIPS OR HOG RINGS AT THIS LOCATION.
3. DAILY INSPECTION SHALL BE MADE BY THE CONTRACTOR AND SILT ACCUMULATION MUST BE REMOVED WHEN DEPTH REACHES 2". INLET PROTECTION SHALL BE REPLACED AS NECESSARY DURING CONSTRUCTION DUE TO DAMAGE OR DETERIORATION (SUBSIDIARY TO INLET PROTECTION).
4. CONTRACTOR SHALL MONITOR THE PERFORMANCE OF INLET PROTECTION DURING EACH RAINFALL EVENT AND ONLY REMOVE INLET PROTECTION IF DIRECTED BY THE CITY OF CORPUS CHRISTI, OR IF CONTRACTOR OBSERVES AN IMMINENT THREAT OF FLOODING OF SURROUNDING PROPERTY.
5. INLET PROTECTIONS SHALL BE REMOVED AS SOON AS THE SOURCE OF SEDIMENT IS STABILIZED.



NOTES:

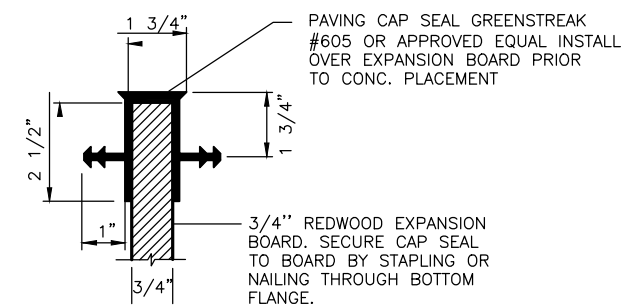
TYPICAL EROSION CONTROL INSTALLATION AT CURB INLET AFTER PLACEMENT OF CURB AND INLET TOP.

CURB INLET PROTECTION DETAIL

NOT TO SCALE

JOINT SEALANT COMPOUND

NOT TO SCALE



CAP SEAL DETAIL NEW CONC. TO NEW CONC.

NOT TO SCALE

*FOR SIDEWALK USE ONLY

REV.	DESCRIPTION	APPR.	DATE



Drawn By :
Checked By : R.T.P.
Approved By : R.T.P.
Project No. : 5740102
Scale : AS NOTED
Date : 10/04/2016
Revision : 0



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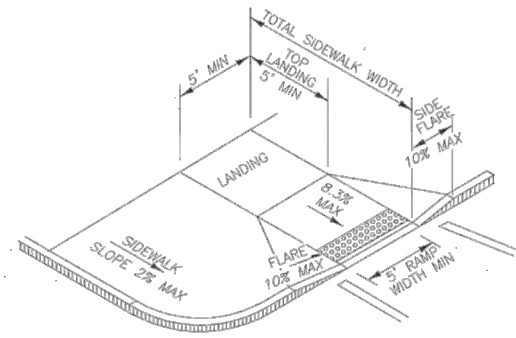
CORPUS CHRISTI REGIONAL
TRANSPORTATION AUTHORITY

ADA BUS STOP IMPROVEMENTS
PHASE VI
MISCELLANEOUS DETAILS
REGIONAL TRANSPORTATION AUTHORITY
CORPUS CHRISTI, TEXAS

Dwg. File:

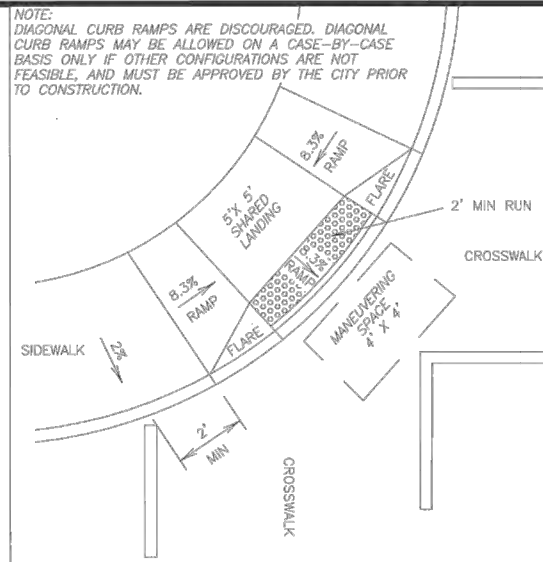
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Sheet 39 of 44



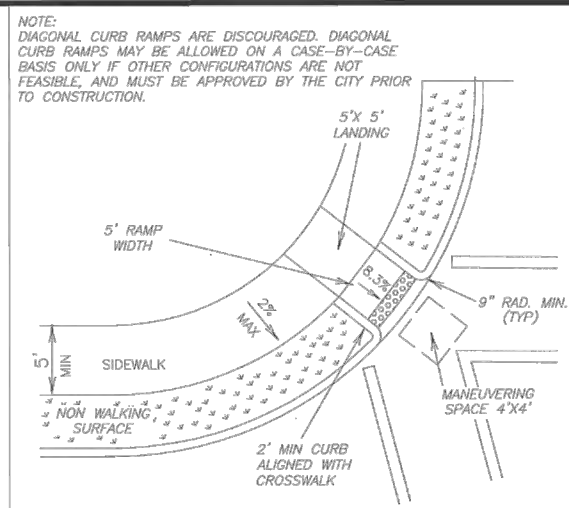
TYPE 1

PERPENDICULAR CURB RAMP
(SIDEWALK ADJACENT TO CURB)



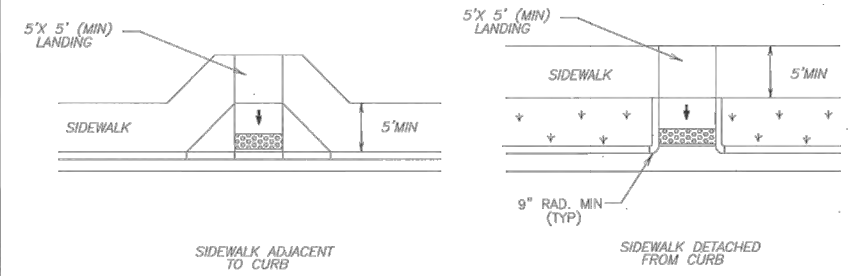
TYPE 4

DIAGONAL COMBINATION CURB RAMP
PERPENDICULAR TO THE TANGENT OF THE CURB
RADIUS AND CONTAINED IN CROSSWALK



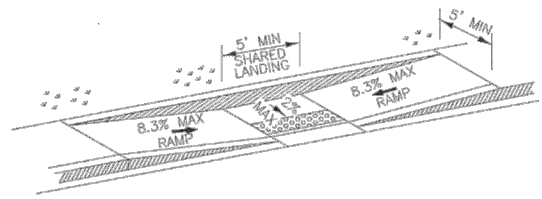
TYPE 7

DIAGONAL CURB RAMP
(RETURNED CURB)



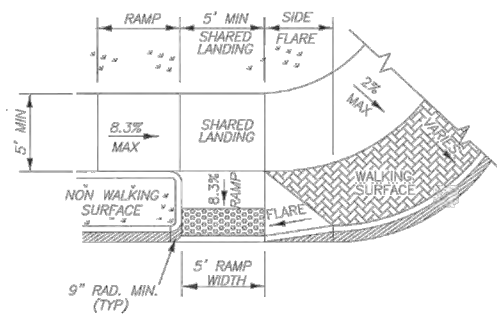
TYPE 10

CURB RAMP MID-BLOCK PLACEMENT
PERPENDICULAR RAMPS



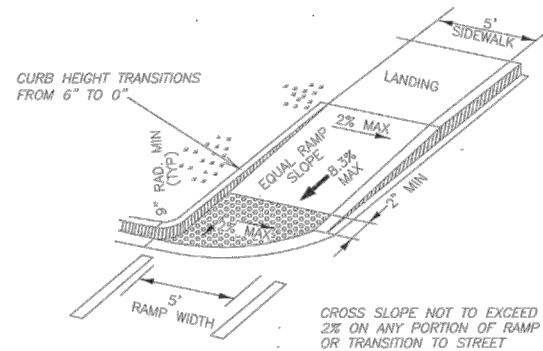
TYPE 2

PARALLEL CURB RAMP WITH
SIDEWALK ADJACENT TO CURB
(USE ONLY WHERE WATER WILL NOT POND IN THE LANDING)



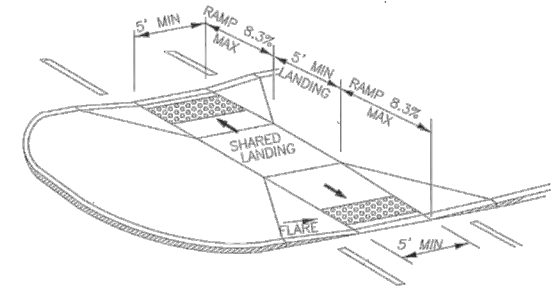
TYPE 5

COMBINATION CURB RAMP



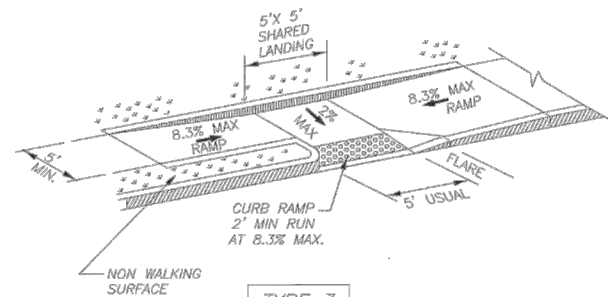
TYPE 8

DIRECTIONAL RAMP WITHIN RADIUS
(SIDEWALK ADJACENT TO CURB)



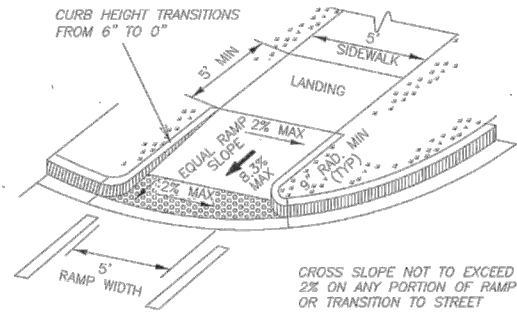
TYPE 11

CURB RAMPS AT MEDIAN ISLAND



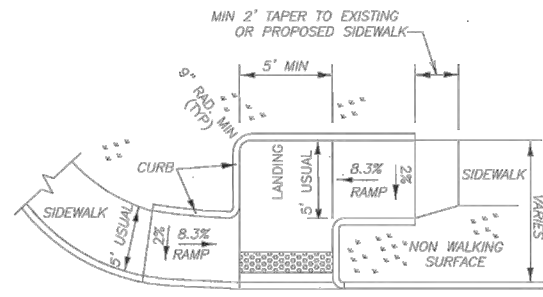
TYPE 3

PARALLEL CURB RAMP



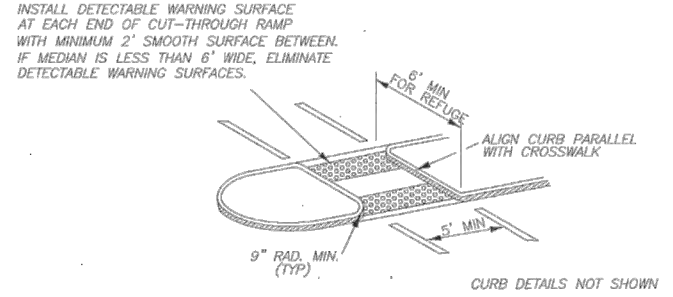
TYPE 6

DIRECTIONAL RAMP WITHIN RADIUS
(SIDEWALK SET BACK FROM CURB)



TYPE 9

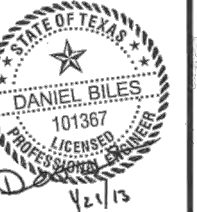
OFFSET PARALLEL CURB RAMP



TYPE 12

FLUSH CURB RAMP AT MEDIAN ISLAND OPENING

CONSULTANT'S JOB NO.



DESCRIPTION

BY

DATE

REVISION NO.

CITY OF
CORPUS CHRISTI
TEXAS

Department of Engineering Services

ADA BUS STOP IMPROVEMENTS
PHASE VI
REGIONAL TRANSPORTATION AUTHORITY

CITY OF CORPUS CHRISTI PEDESTRIAN
CURB RAMP STANDARDS

1 OF 4

DESCRIPTION

BY

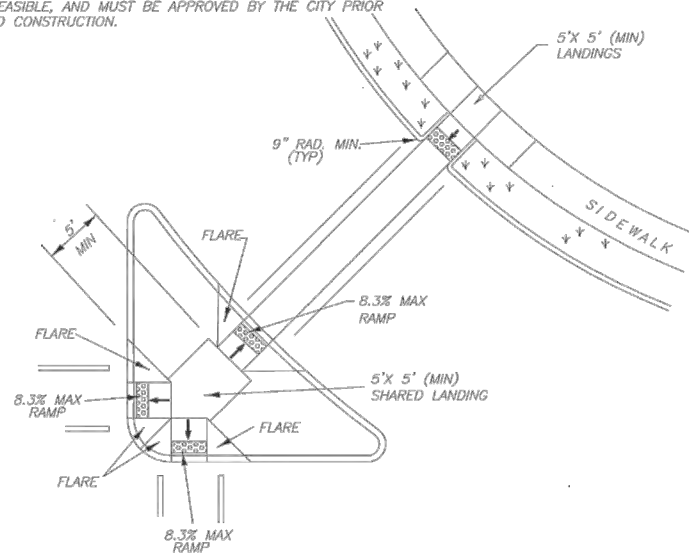
DATE

REVISION NO.

SHEET 40 of 44
RECORD DRAWING NO.

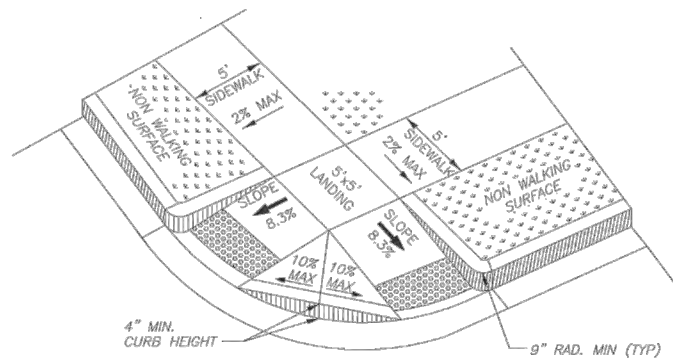
CITY PROJECT #

NOTE:
DIAGONAL CURB RAMPS ARE DISCOURAGED. DIAGONAL CURB RAMPS MAY BE ALLOWED ON A CASE-BY-CASE BASIS ONLY IF OTHER CONFIGURATIONS ARE NOT FEASIBLE, AND MUST BE APPROVED BY THE CITY PRIOR TO CONSTRUCTION.



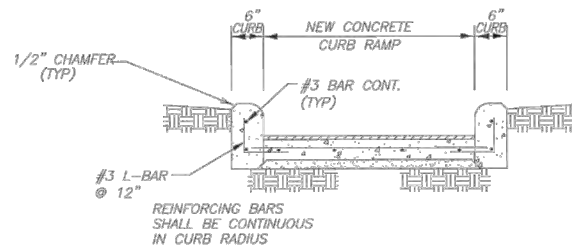
TYPE 13

AT INTERSECTION W/FREE RIGHT TURN & ISLAND
COMBINATION ISLAND RAMPS



TYPE 14

PERPENDICULAR CURB RAMPS (BI-DIRECTIONAL)
(SIDEWALK SET BACK FROM CURB)



HEADER CURBS AT CURB RAMP

SUBGRADE PREPARATION:

1. SUBGRADE UNDER CONCRETE SIDEWALKS AND CURB RAMPS SHALL BE COMPACTED TO 95% STANDARD PROCTOR DENSITY.

SIDEWALK NOTES:

1. THE MINIMUM SIDEWALK WIDTH FOR ALL ARTERIAL AND COLLECTOR STREETS IS 5'. WHERE A 5' SIDEWALK CAN NOT BE PROVIDED DUE TO SITE CONSTRAINTS, A MINIMUM 4' SIDEWALK MAY BE PROVIDED. 5'X5' PASSING AREAS SHALL BE PROVIDED AT INTERVALS NOT TO EXCEED 200' FOR ALL SIDEWALKS LESS THAN 5' IN WIDTH.
2. MAXIMUM ALLOWABLE CROSS SLOPE ON SIDEWALK SURFACE IS 2%.
3. ALL EXPANSION JOINTS TO BE 3/4" THICK WOOD FIBER ASPHALT-IMPREGNATED EXPANSION BOARD, UNLESS OTHERWISE NOTED.
4. ALL CONCRETE TO BE CLASS 'A' $f'_c=3,000$ PSI. ALL REINFORCING STEEL TO BE GRADE 60, $f_y=60,000$ PSI.
5. SIDEWALKS SHALL BE AT LEAST 4" THICK CONCRETE.
6. CONCRETE SURFACE TO RECEIVE BROOM FINISH.
7. TRANSVERSE CONTRACTION JOINTS 1/8" WIDE BY 1/2" DEEP SHALL BE CUT IN ALL SIDEWALKS AT 5'-0" INTERVALS (MAXIMUM).
8. PROVIDE PEDESTRIAN ACCESSIBLE ROUTE WITH DETECTABLE WARNING SURFACE FOR SIDEWALKS THAT INTERSECT CONTROLLED DRIVEWAYS. DETECTABLE WARNING SURFACE SHALL BE A MINIMUM OF 24" IN DEPTH IN THE DIRECTION OF PEDESTRIAN TRAVEL, AND EXTEND THE FULL WIDTH OF THE ACCESSIBLE ROUTE WHERE IT INTERSECTS THE CONTROLLED DRIVEWAY.

CURB RAMP NOTES:

1. PROVIDE CURB RAMPS WHEREVER AN ACCESSIBLE ROUTE CROSSES (PENETRATES) A CURB.
2. SLOPE CRITERIA

RAMPS AND LANDING AREAS

	MAX SLOPE (V:H, %, IN PER FT)
RAMP IN DIRECTION OF TRAVEL	1:12 / 8.03% / 1" PER FT
SIDE SLOPE OF RAMP (FLARE)	1:10 / 10% / 1.2" PER FT
CROSS SLOPE OF RAMP	1:50 / 2% / 0.24" PER FT
LANDING AREA (ALL DIRECTIONS)	1:50 / 2% / 0.24" PER FT

ADJOINING AREAS

SIDEWALK IN DIRECTION OF TRAVEL	1:20 / 5% / 0.60" PER FT
SIDEWALK CROSS SLOPE	1:50 / 2% / 0.24" PER FT
GUTTER IN DIRECTION OF TRAVEL	1:20 / 5% / 0.60" PER FT

A SMOOTH TRANSITION ($S \leq 1:50$) IN DIRECTION OF TRAVEL IS REQUIRED WHERE RAMPS TRANSITION TO THE STREET

3. PROVIDE FLARED SIDES WHERE THE PEDESTRIAN CIRCULATION PATH CROSSES THE CURB RAMP. FLARED SIDES SHALL BE SLOPED AT 10% MAXIMUM, MEASURED PARALLEL TO THE CURB. RETURNED CURBS MAY BE USED ONLY WHERE PEDESTRIANS WOULD NOT NORMALLY WALK ACROSS THE RAMP, EITHER BECAUSE THE ADJACENT SURFACE IS PLANTED, SUBSTANTIALLY OBSTRUCTED, OR OTHERWISE PROTECTED.
4. LANDINGS SHALL BE 5'X5' MINIMUM WITH A MAXIMUM 2% SLOPE IN ANY DIRECTION.
5. CURB RAMP MUST BE WHOLLY CONTAINED WITHIN CROSSWALK MARKINGS, EXCLUDING SIDE FLARES.

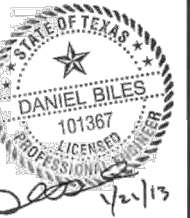
CURB RAMP NOTES (CONTINUED):

6. CURB RAMPS, FLARES AND LANDINGS SHALL BE AT LEAST 5" THICK CONCRETE AND EXPANSION JOINTS SHALL TYPICALLY BE USED AT MATCHLINE WITH ADJOINING AREAS.
7. MANEUVERING SPACE AT THE BOTTOM OF CURB RAMPS SHALL BE A MINIMUM OF 4'X4' WHOLLY CONTAINED WITHIN THE CROSSWALK AND WHOLLY OUTSIDE THE PARALLEL VEHICULAR TRAVEL PATH.
8. LAYBACK CURB AND GUTTER MAY BE CONSTRUCTED MONOLITHICALLY WITH CURB RAMPS. PROVIDE NO. 4 X 12" LONG SMOOTH DOWELS @ 12" ON CENTERS IF NOT PLACED MONOLITHICALLY.
9. PROVIDE A SMOOTH TRANSITION WHERE THE CURB RAMPS CONNECT TO THE STREET. 5% MAXIMUM SLOPE IN GUTTER.
10. ADDITIONAL INFORMATION ON CURB RAMP LOCATION, DESIGN, LIGHT REFLECTIVE VALUE AND TEXTURE MAY BE FOUND IN THE CURRENT EDITION OF THE TEXAS ACCESSIBILITY STANDARDS (TAS) AND 16 TAC §68.102.
11. DIAGONAL CURB RAMPS ARE DISCOURAGED. DIAGONAL CURB RAMPS MAY BE ALLOWED ON A CASE-BY-CASE BASIS ONLY IF OTHER CONFIGURATIONS ARE NOT FEASIBLE, AND MUST BE APPROVED BY THE CITY PRIOR TO CONSTRUCTION.
12. FINAL ACCEPTANCE OF THE PROJECT SHALL BE CONTINGENT UPON THE CONTRACTOR PROVIDING THE CITY WITH A FINAL INSPECTION REPORT FROM A CERTIFIED REGISTERED ACCESSIBILITY SPECIALIST (RAS) PER 16 TAC §68.52 STATING THAT ALL ADA (AMERICANS WITH DISABILITIES ACT) HANDICAP IMPROVEMENTS, AS CONSTRUCTED, COMPLY WITH THE TEXAS ACCESSIBILITY STANDARDS (TAS) FOR ELIMINATION OF ARCHITECTURAL BARRIERS PER TEXAS GOVERNMENT CODE CHAPTER 469.

DETECTABLE WARNING SURFACE NOTES:

1. CURB RAMPS MUST CONTAIN A DETECTABLE WARNING SURFACE THAT CONSISTS OF RAISED TRUNCATED DOMES COMPLYING WITH SECTION 705 OF THE TAS. THE SURFACE MUST CONTRAST VISUALLY WITH ADJOINING SURFACES INCLUDING SIDE FLARES.
2. DETECTABLE WARNING SURFACE FOR RAMPS SHALL BE ADA SOLUTIONS, INC. PART NO. 2460REP CAST-IN-PLACE REPLACEABLE TACTILE WARNING SURFACE TILES TRUNCATED DOME, OR APPROVED EQUIVALENT, IN "BRICK RED" COLOR.
3. ALIGN TRUNCATED DOMES IN THE DIRECTION OF PEDESTRIAN TRAVEL WHEN ENTERING THE STREET.
4. DETECTABLE WARNING SURFACES SHALL BE A MINIMUM OF 24" IN DEPTH IN THE DIRECTION OF PEDESTRIAN TRAVEL, AND EXTEND THE FULL WIDTH OF THE CURB RAMP OR LANDING WHERE THE PEDESTRIAN ACCESS ROUTE ENTERS THE STREET.
5. DETECTABLE WARNING SURFACES SHALL BE LOCATED SO THAT THE EDGE NEAREST THE CURB LINE IS A MINIMUM OF 6" AND A MAXIMUM OF 10" FROM THE EXTENSION OF THE FACE OF CURB. DETECTABLE WARNING SURFACES TO BE CURVED ALONG THE CORNER RADIUS.

CONSULTANT'S JOB NO.



DESCRIPTION

BY

DATE

REVISION NO.

CITY OF
CORPUS CHRISTI
TEXAS



Department of Engineering Services

ADA BUS STOP IMPROVEMENTS
PHASE VI
REGIONAL TRANSPORTATION AUTHORITY

CITY OF CORPUS CHRISTI PEDESTRIAN
CURB RAMP STANDARDS

2 OF 4

DESCRIPTION

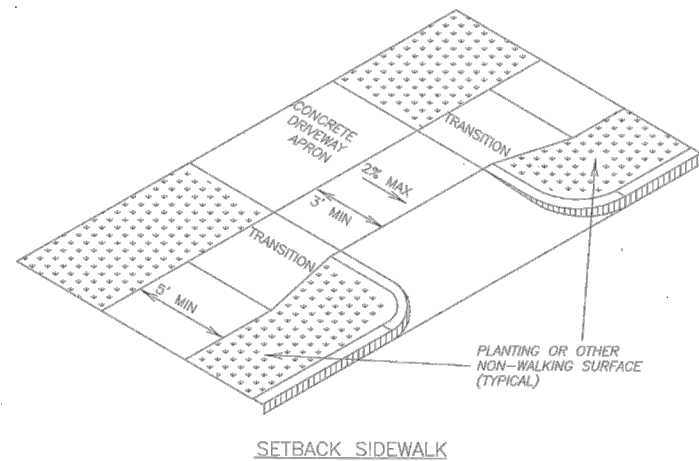
BY

DATE

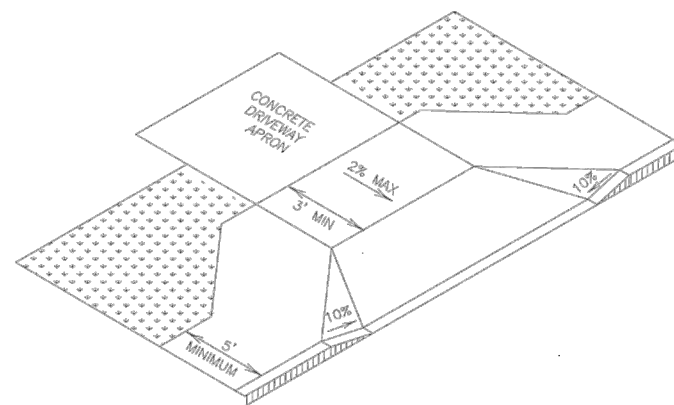
REVISION NO.

SHEET 41 of 44
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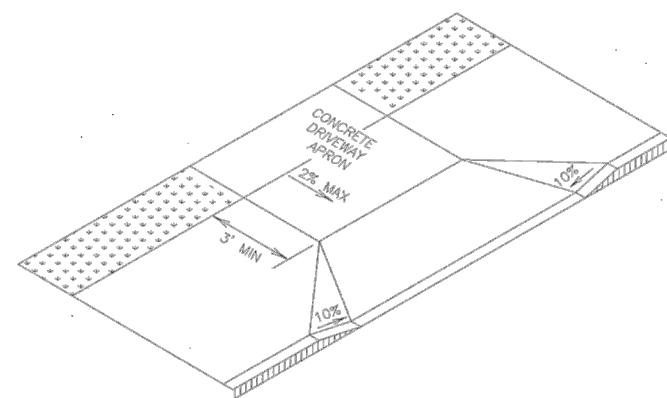
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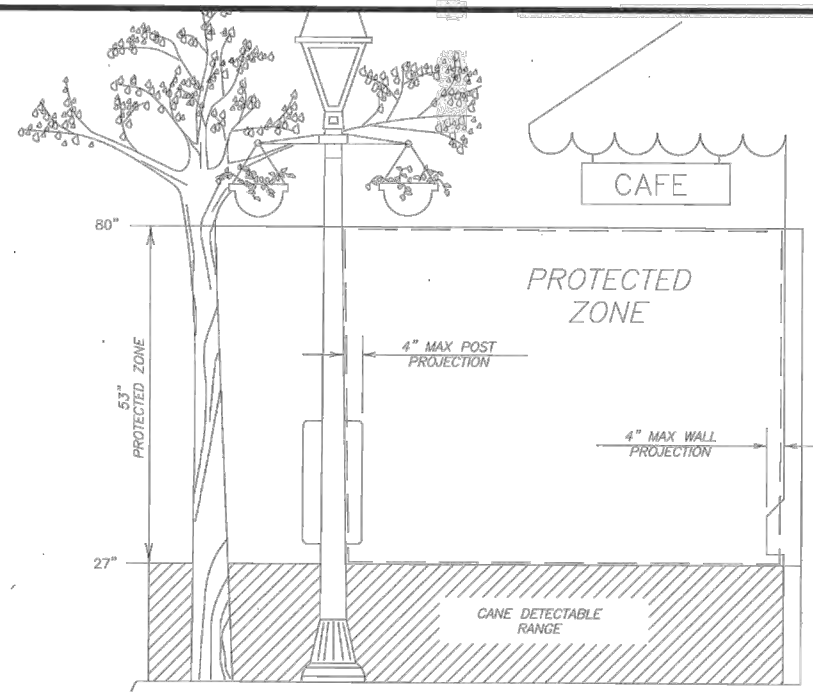
SETBACK SIDEWALK



APRON OFFSET SIDEWALK

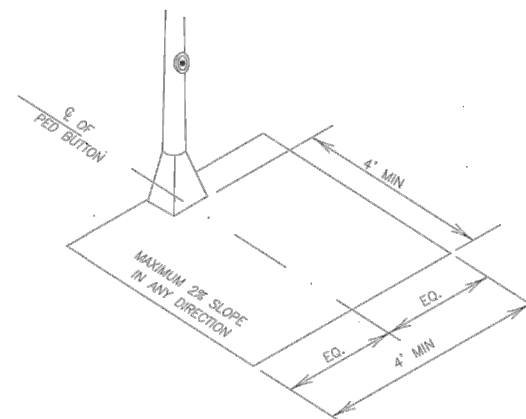


WIDE SIDEWALK
SIDEWALK TREATMENT AT DRIVEWAYS

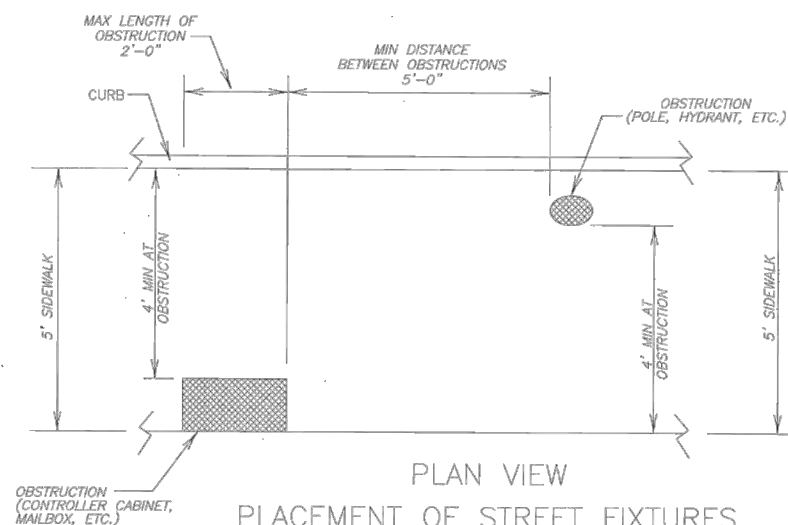


PROTECTED ZONE

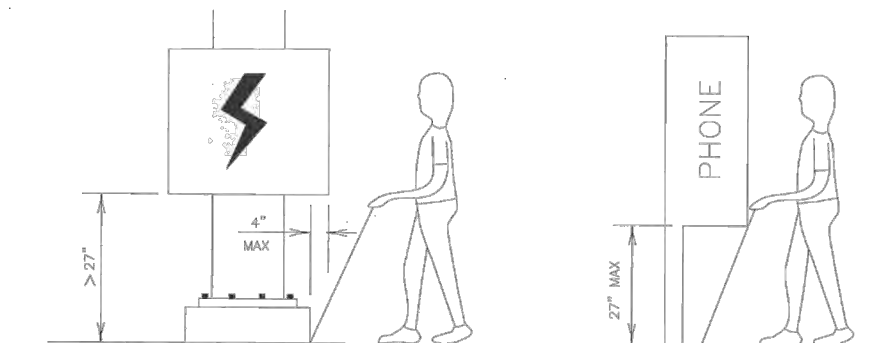
IN PEDESTRIAN CIRCULATION AREA, MAXIMUM 4" PROJECTION FOR POST OR WALL MOUNTED OBJECTS BETWEEN 27" AND 80" ABOVE THE SURFACE.



CLEAR GROUND SPACE CENTERED AT PEDESTRIAN PUSH BUTTON



PLAN VIEW
PLACEMENT OF STREET FIXTURES
(ITEMS NOT INTENDED FOR PUBLIC USE. MINIMUM 4' X 4' CLEAR GROUND SPACE REQUIRED AT PUBLIC USE FIXTURES.)



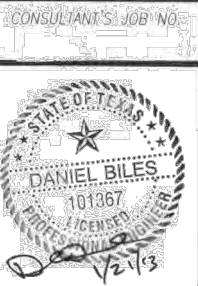
WHEN AN OBSTRUCTION OF A HEIGHT GREATER THAN 27" FROM THE SURFACE WOULD CREATE A PROTRUSION OF MORE THAN 4" INTO THE PEDESTRIAN CIRCULATION AREA, CONSTRUCT ADDITIONAL CURB OR FOUNDATION AT THE BOTTOM TO PROVIDE A MAXIMUM 4" OVERHANG.

PROTRUDING OBJECTS OF A HEIGHT < 27" ARE DETECTABLE BY CANE AND DO NOT REQUIRE ADDITIONAL TREATMENT.

DETECTION BARRIER FOR VERTICAL CLEARANCE < 80"

GENERAL NOTES

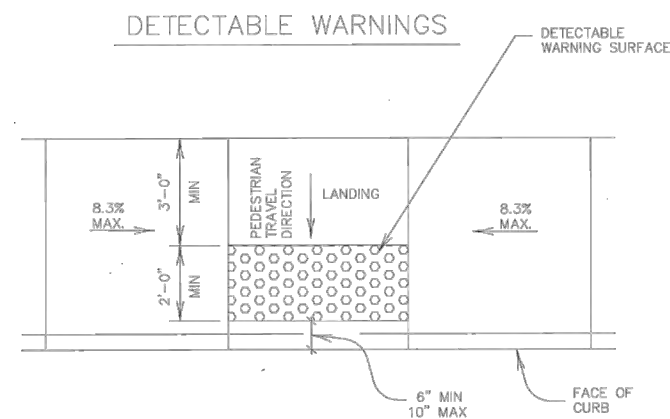
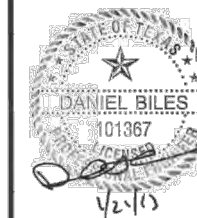
1. ALL SLOPES ARE MAXIMUM ALLOWABLE. THE LEAST POSSIBLE SLOPE THAT WILL STILL DRAIN PROPERLY SHOULD BE USED.
2. PLACE TRAFFIC SIGNAL OR ILLUMINATION POLES, GROUND BOXES, CONTROLLER BOXES, SIGNS, DRAINAGE FACILITIES AND OTHER ITEMS SO AS NOT TO OBSTRUCT THE ACCESSIBLE ROUTE OR CLEAR GROUND SPACE.
3. THE MAXIMUM ALLOWABLE SIDEWALK CROSS SLOPE EQUALS 2%.
4. STREET GRADES AND CROSS SLOPES SHALL BE AS SHOWN ELSEWHERE IN THE PLANS.
5. EXISTING FEATURES THAT COMPLY WITH TAS MAY REMAIN IN PLACE UNLESS OTHERWISE SHOWN ON THE PLANS.
6. CHANGES IN LEVEL GREATER THAN 1/4 INCH ARE NOT PERMITTED.
7. THE LEAST POSSIBLE GRADE SHOULD BE USED TO MAXIMIZE ACCESSIBILITY. THE RUNNING SLOPE OF SIDEWALKS AND CROSSWALKS, WITHIN THE PUBLIC RIGHT-OF-WAY, MAY FOLLOW THE GRADE OF THE PARALLEL ROADWAY. WHERE A CONTINUOUS GRADE GREATER THAN 5% MUST BE PROVIDED, HANDRAILS MAY BE DESIRABLE ON ONE OR BOTH SIDES OF THE SIDEWALK TO IMPROVE ACCESSIBILITY. HANDRAILS MAY ALSO BE NEEDED TO PROTECT PEDESTRIANS FROM POTENTIALLY HAZARDOUS CONDITIONS. IF PROVIDED, HANDRAILS MUST COMPLY WITH TAS 4.8.5.
8. HANDRAIL EXTENSIONS SHALL NOT PROTRUDE INTO THE USABLE LANDING AREA OR INTO INTERSECTING PEDESTRIAN ROUTES.
9. SIDEWALK DETAILS ARE SHOWN ELSEWHERE IN THE PLANS.



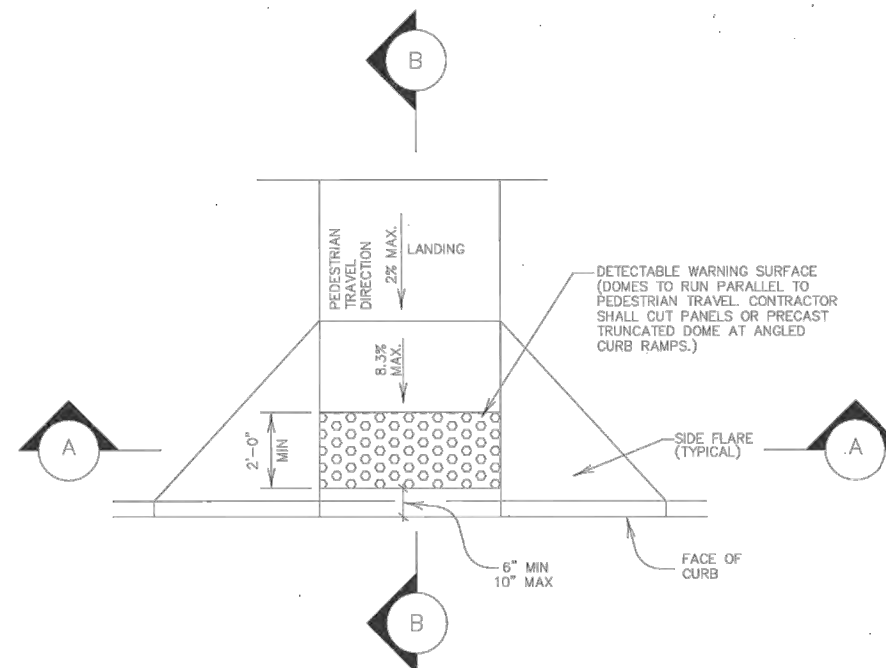
DESCRIPTION
DATE
REVISION NO.
CITY OF
CORPUS CHRISTI
TEXAS
Department of Engineering Services

ADA BUS STOP IMPROVEMENTS
PHASE VI
REGIONAL TRANSPORTATION AUTHORITY
CITY OF CORPUS CHRISTI PEDESTRIAN
CURB RAMP STANDARDS
3 OF 4

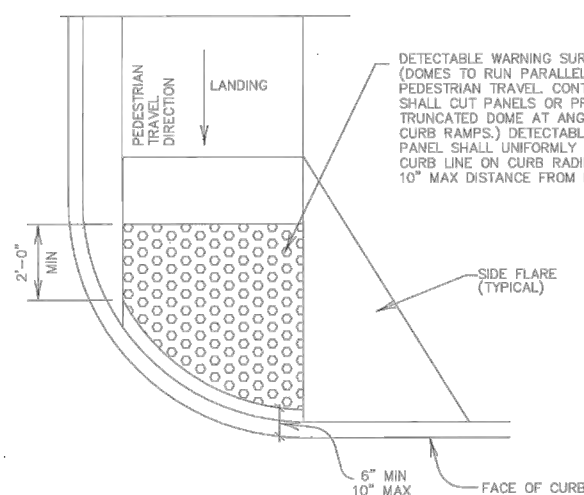
SHEET 42 of 44
RECORD DRAWING NO.
CITY PROJECT #6485



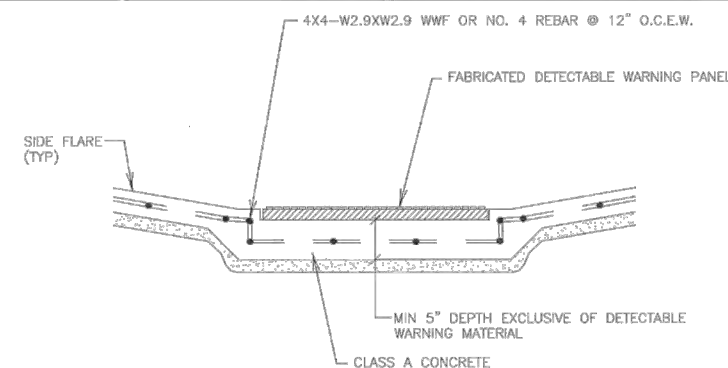
PARALLEL CURB RAMP
TYPICAL PLACEMENT OF DETECTABLE WARNING SURFACE ON LANDING AT STREET EDGE



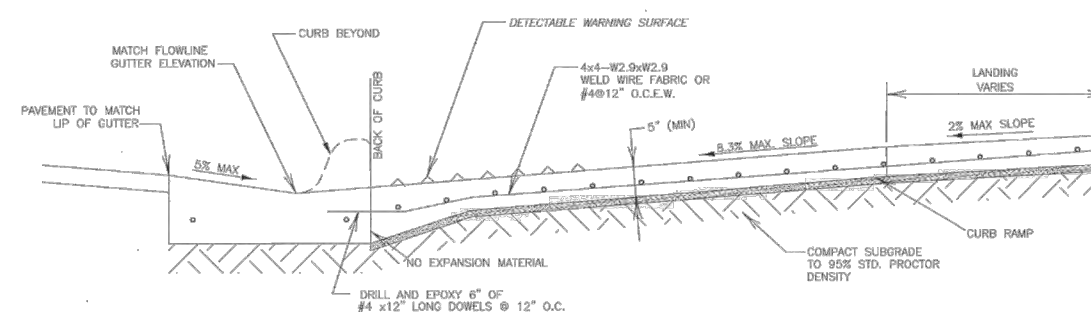
PERPENDICULAR CURB RAMP
TYPICAL PLACEMENT OF DETECTABLE WARNING SURFACE ON SLOPING RAMP RUN



DIRECTIONAL CURB RAMP
TYPICAL PLACEMENT OF DETECTABLE WARNING SURFACE ON SLOPING RAMP RUN AT A RADIUS



SECTION A-A
TYPICAL SECTION THROUGH CURB RAMP



SECTION B-B
CURB RAMP PROFILE

DESCRIPTION

BY

DATE

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CITY OF
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Department of Engineering Services

ADA BUS STOP IMPROVEMENTS
PHASE VI
REGIONAL TRANSPORTATION AUTHORITY

CITY OF CORPUS CHRISTI PEDESTRIAN
CURB RAMP STANDARDS 4 OF 4

DESCRIPTION

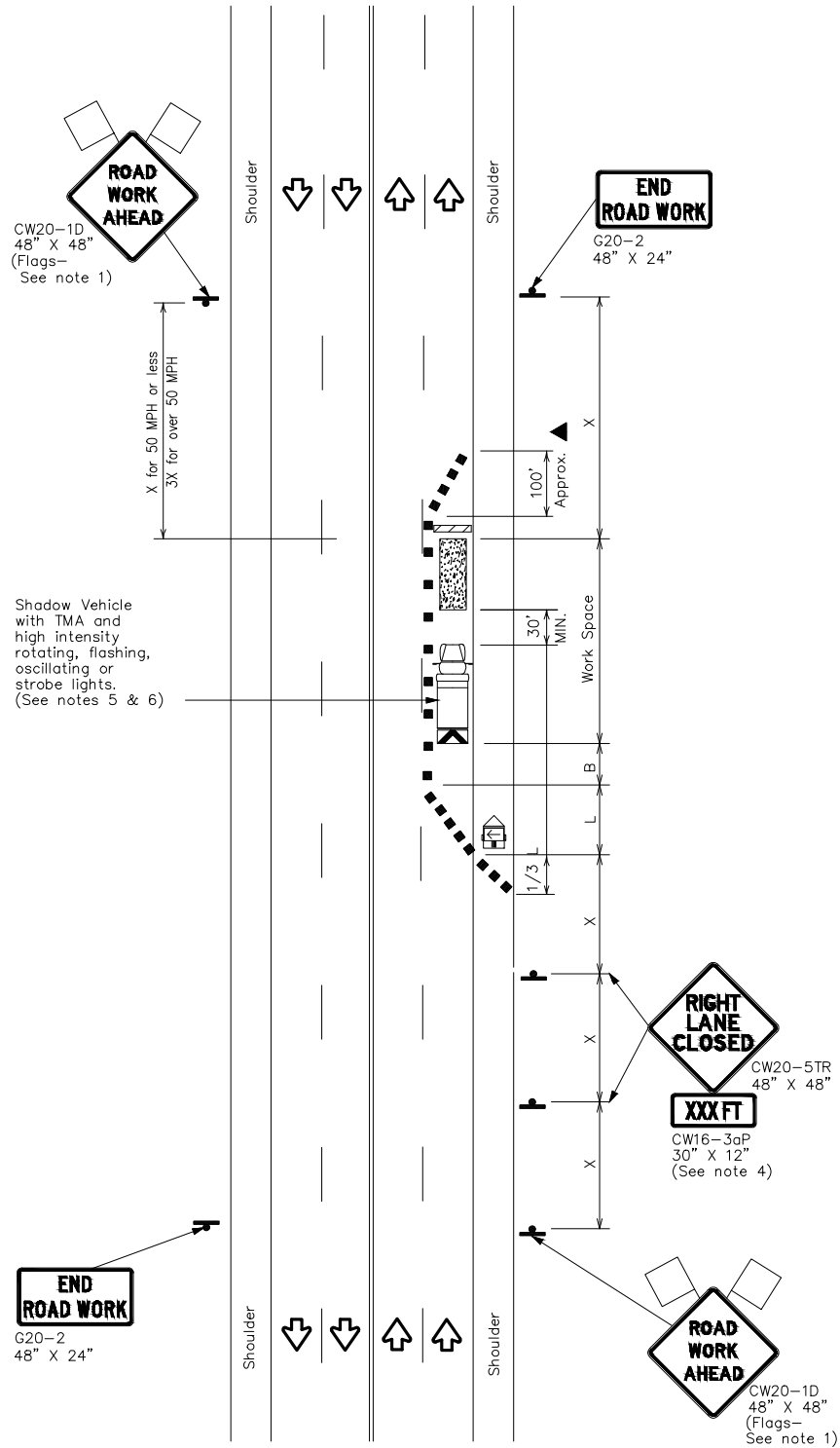
BY

DATE

REVISION NO.

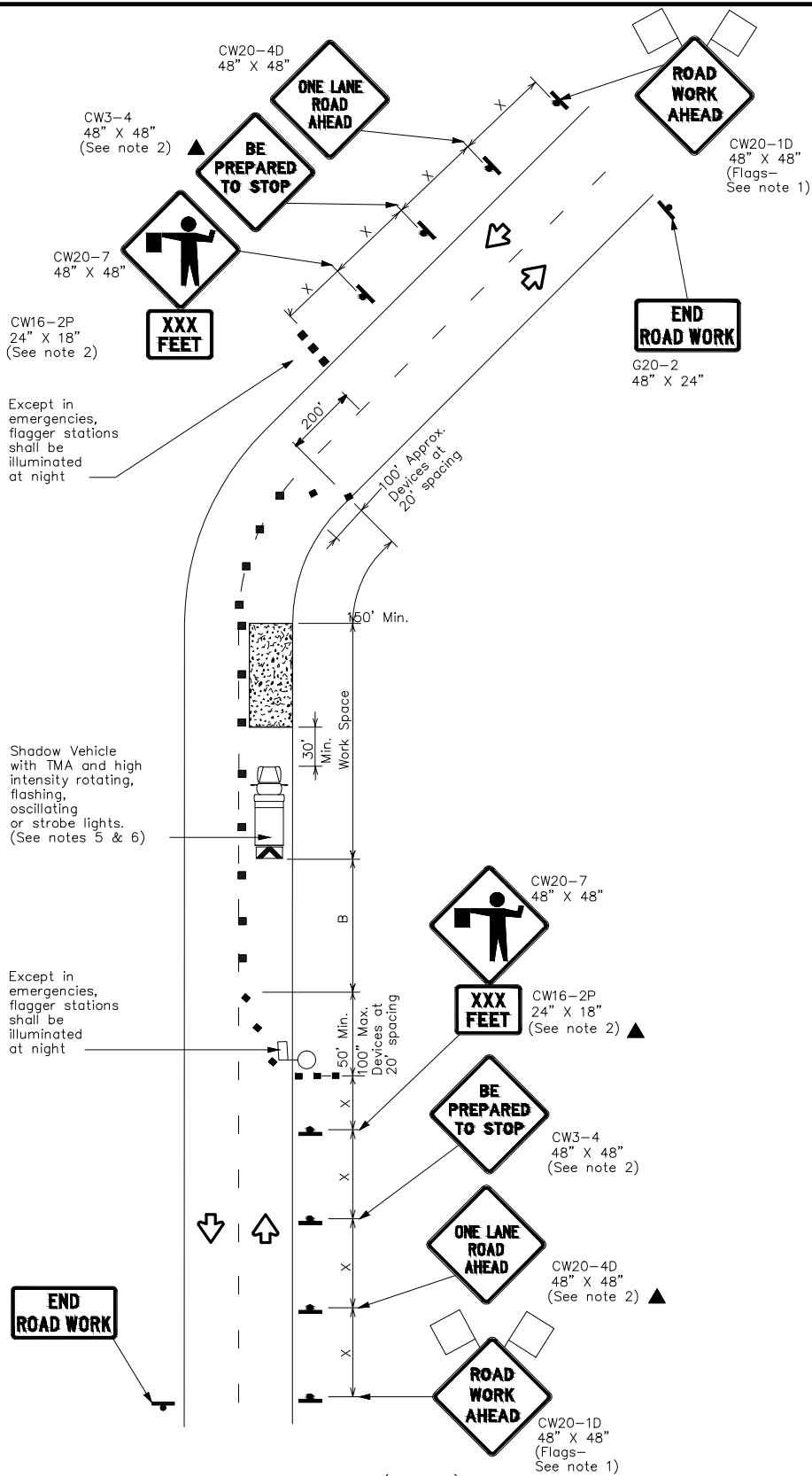
SHEET 43 of 44
RECORD DRAWING NO.

CITY PROJECT #



TCP (2-4)

ONE LANE CLOSED



TCP (1-2b)

ONE LANE TWO-WAY
CONTROL WITH FLAGGERS

LEGEND			
	Type 3 Barricade		Channelizing Devices
	Heavy Work Vehicle		Truck Mounted Attenuator (TMA)
	Trailer Mounted Flashing Arrow Board		Portable Changeable Message Sign (PCMS)
	Sign		Traffic Flow
	Flag		Flagger

Posted Speed *	Formula	Minimum Desirable Taper Lengths **			Suggested Maximum Spacing of Channelizing Devices		Minimum Sign Spacing "x" Distance	Suggested Longitudinal Buffer Space "B"
		10' Offset	11' Offset	12' Offset	On a Taper	On a Tangent		
30	$L = \frac{WS^2}{60}$	150'	165'	180'	30'	60'	120'	90'
35		205'	225'	245'	35'	70'	160'	120'
40		265'	295'	320'	40'	80'	240'	155'
45	L=WS	450'	495'	540'	45'	90'	320'	195'
50		500'	550'	600'	50'	100'	400'	240'
55		550'	605'	660'	55'	110'	500'	295'
60		600'	660'	720'	60'	120'	600'	350'
65		650'	715'	780'	65'	130'	700'	410'
70		700'	770'	840'	70'	140'	800'	475'
75		750'	825'	900'	75'	150'	900'	540'

* Conventional Roads Only
** Taper lengths have been rounded off.
L=Length of Taper(FT) W=Width of Offset(FT) S=Posted Speed(MPH)

TYPICAL USAGE				
MOBILE	SHORT DURATION	SHORT TERM STATIONARY	INTERMEDIATE TERM STATIONARY	LONG TERM STATIONARY
	✓	✓	✓	

GENERAL NOTES

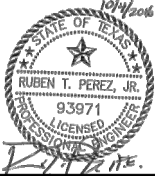
- Flags attached to signs where shown, are REQUIRED.
- All traffic control devices illustrated are REQUIRED, except those denoted with the triangle symbol may be omitted when stated elsewhere in the plans, or for routine maintenance work, when approved by the Engineer.
- The downstream taper is optional. When used, it should be 100 feet minimum length per lane.
- For short term applications, when post mounted signs are not used, the distance legend may be shown on the sign face rather than on a CW16-3aP supplemental plaque.
- A Shadow Vehicle with a TMA should be used anytime it can be positioned 30 to 100 feet in advance of the area of crew exposure without adversely affecting the performance or quality of the work. If workers are no longer present but road or work conditions require the traffic control to remain in place, Type 3 Barricades or other channelizing devices may be substituted for the Shadow Vehicle and TMA.
- Additional Shadow Vehicles with TMAs may be positioned in each closed lane, on the shoulder or off the paved surface, next to those shown in order to protect a wider work space.

TCP (2-4a)

- If this TCP is used for a left lane closure, CW20-5TL "LEFT LANE CLOSED" signs shall be used and channelizing devices shall be placed on the centerline to protect the work space from opposing traffic with the arrow board placed in the closed lane near the end of the merging taper.

For construction or maintenance contract work, specific project requirements for shadow vehicles can be found in the project GENERAL NOTES for Item 502, Barricades, Signs and Traffic Handling.

REV.	DESCRIPTION	APPR.	DATE



Drawn By	: A.N.
Checked By	: R.T.P.
Approved By	: R.T.P.
Project No.	: 5740102
Scale	: AS NOTED
Date	: 10/04/2016
Revision	: 0



Martinez, Guy & Maybik, Inc.
Civil / Structural Engineering & Surveying Services
TX. Bd. P.E. Firm Reg. No. F-1415

6000 S. Staples
Suite. 207
Corpus Christi
Tx. 78413
P: 361-814-3070
Fax: 888-653-5510



CORPUS CHRISTI REGIONAL
TRANSPORTATION AUTHORITY

ADA BUS STOP IMPROVEMENTS
PHASE VI
TRAFFIC CONTROL PLAN
REGIONAL TRANSPORTATION AUTHORITY
CORPUS CHRISTI, TEXAS

Dwg. File:

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